



City of Nedlands

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PD44.13 - Attachment 1 - Locality Plan

1:400



1A 850051

DRAWING

IT SHOULD BE NOTED THAT UNDERGROUND SERVICES ARE SHOWN BUT NEED TO BE VERIFIED BY THE RELEVANT AUTHORITY PRIOR TO ANY DEMOLITION, EXCAVATION OR CONSTRUCTION. THEREFORE, NO RESPONSIBILITY IS TAKEN FOR SERVICE LOCATIONS.

NOTE: THIS PLAN IS ACCURATE AT TIME OF SURVEY. HORIZON SURVEYS ACCEPTS NO RESPONSIBILITY FOR THE ACCURACY OF ANY INFORMATION ADDED.

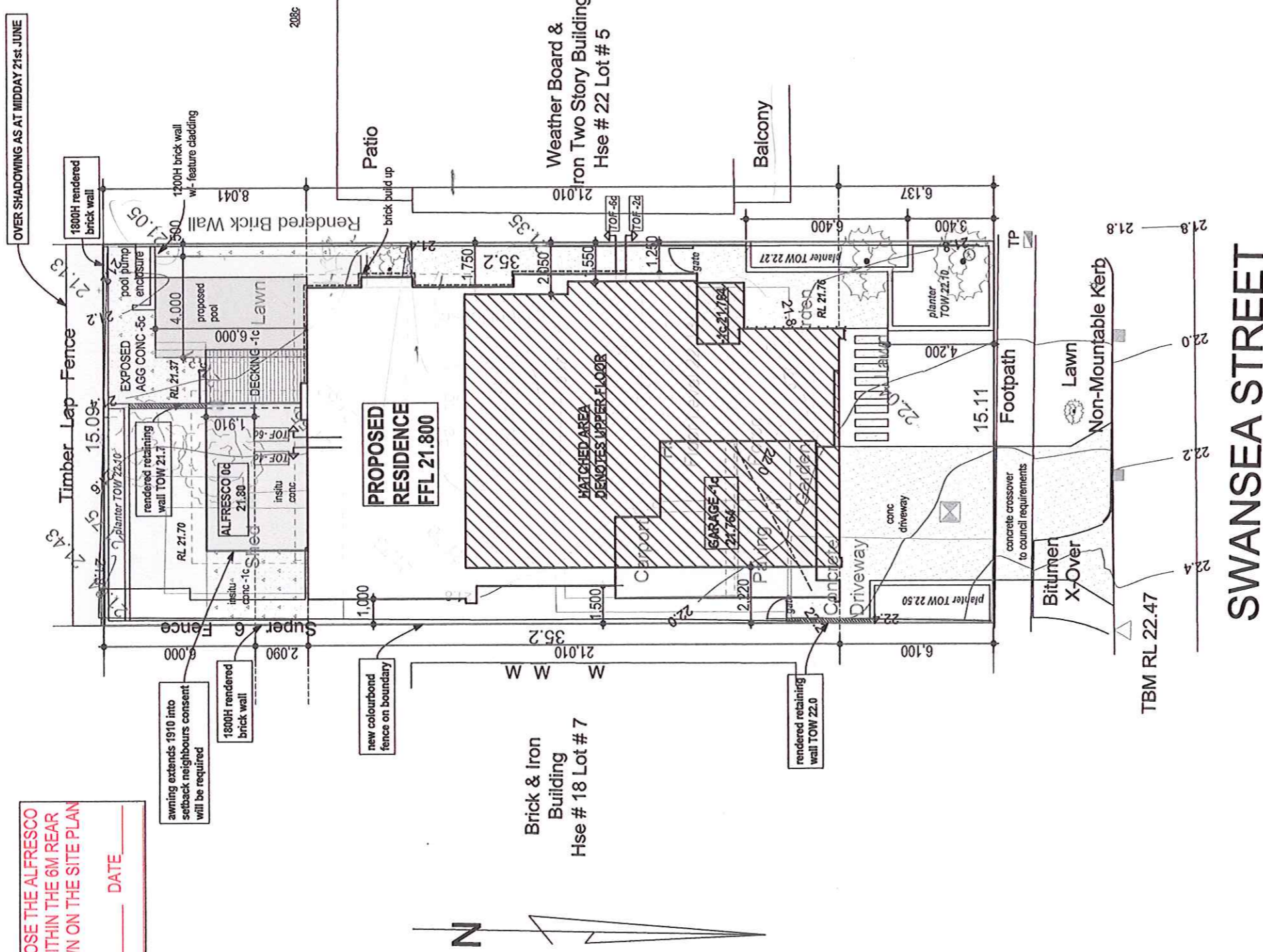
I DO/DO NOT OPPOSE THE ALFRESCO ENCROACHING WITHIN THE 6M REAR SETBACK AS SHOWN ON THE SITE PLAN

SIGNED _____ DATE _____
ADDRESS _____

SCALE



LOCAL AUTHORITY: City of Nedlands



SITE PLAN

Legend

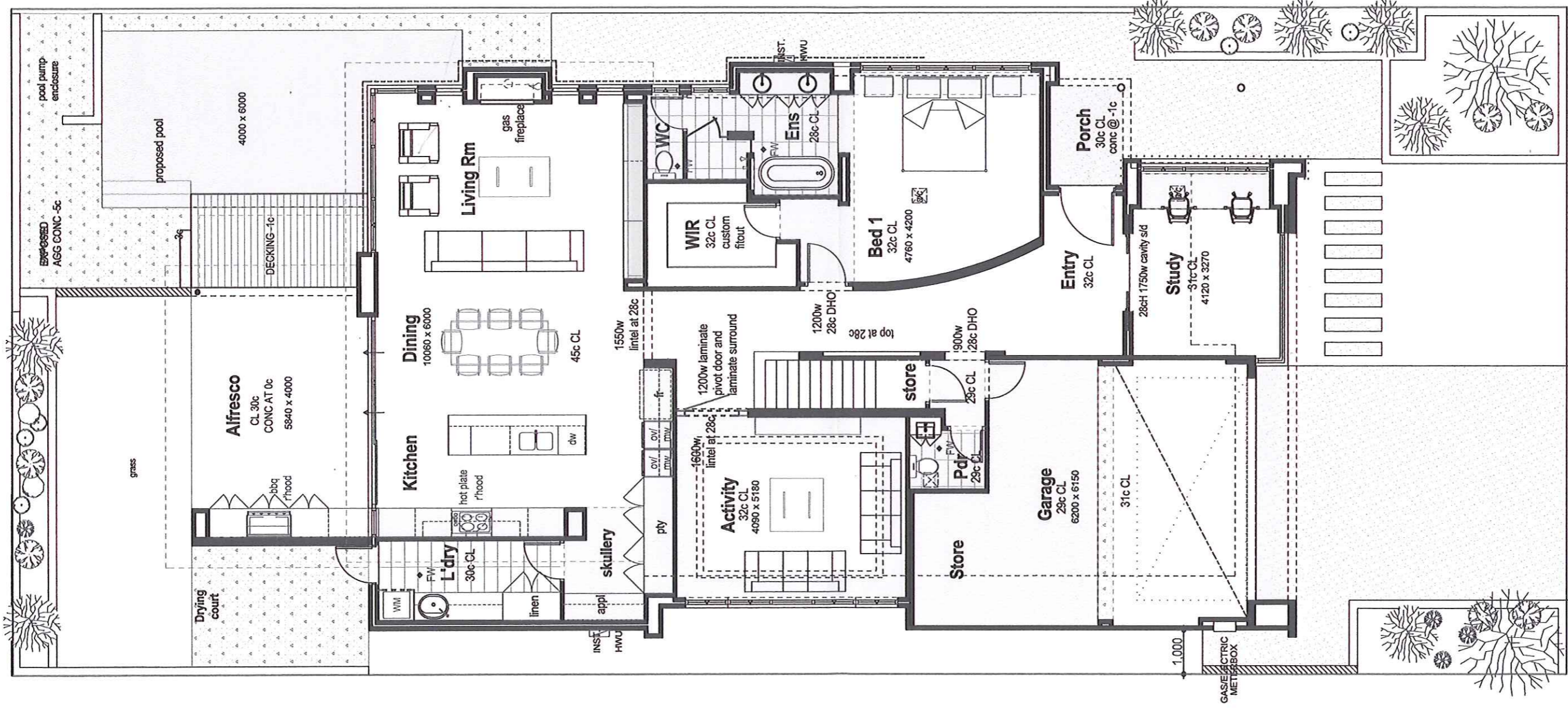
- Gas Meter
- Water Meter
- Water Tap
- Gully Trap
- Water Bore
- Telecom Pit
- Sewer Vent
- Sewer Manhole
- Power Pole
- Power Pole Domestic
- Light Pole
- Clothes Hoist
- Shrub
- Palm
- Tree

6 AUG 2013

CHECK TITLE FOR ANY ENCUMBRANCES AREA AND DIMENSIONS ARE SUBJECT TO SURVEY DIMENSIONS DERIVED FROM CERTIFICATE OF TITLE OR SURVEY PLAN

ALL CADASTRAL INFORMATION SHOWN IS THE RESULT OF THE LOCATION OF LIMITED SURVEY MARKS AND AS SUCH BOUNDARY LOCATIONS COULD VARY UPON FINAL SURVEY

<p>HORIZON SURVEYS LEVEL 1/61 MARLOW ST. WEMBLEY 6014 PH 9387 8226 Fax 9387 8284 Mobile 0407 080 073 Email peterb@horizonsurveys.com.au</p>	CLIENT	PETER CORNER
	TITLE	FEATURE SURVEY 20 SWANSEA STREET, SWANBOURNE (LOT 6 on PLAN 227)
SURVEYED P/B/EG	DATE	JOB No.
COMPUTED EG	23/05/13	130058
DRAWN EG	23/05/13	HORIZONTAL DATUM LOCAL
CHECKED	24/05/13	FIELD BOOK 98
		DRAWING No. 130058_V1
		SCALE 1:200 at A3
		VERTICAL DATUM AHD (ex Wat Corp)
		LEVEL BOOK



Ground Floor	A: 201.11 m ²
	P: 71.89 m
Alfresco	A: 23.19 m ²
Garage	A: 49.34 m ²
Porch	A: 4.33 m ²
Upper Floor	A: 140.51 m ²
	P: 49.42 m
Balcony	A: 17.80 m ²
Total Area	436.28m²

Site Cover 250.45m² 47%

CITY OF NEDLANDS
16 AUG 2013

PROPOSED RESIDENCE FOR P & B CORNER #20 SWANSEA STREET, SWANBOURNE

rachel roberts
residential designer

DRAWN 20/7/2013

Total Area 436.28m²

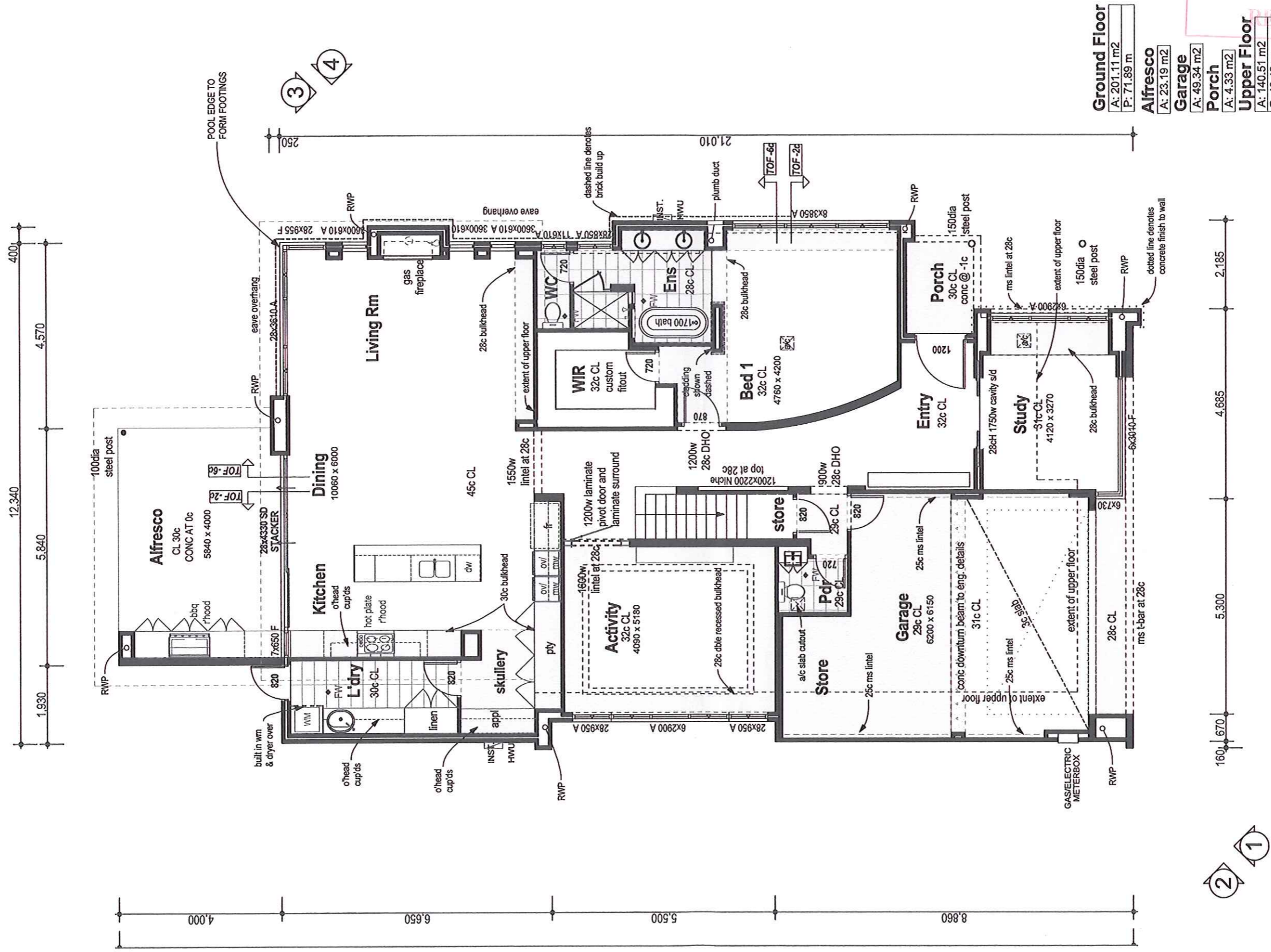
Site Cover 250.45m² 47%



GROUND STOREY PLAN

PROPOSED RESIDENCE FOR P & B CORNER
#20 SWANSEA STREET, SWANBOURNE

DRAWN 20/7/2013



Ground Floor	A: 201.11 m ²
	P: 71.89 m
Alfresco	A: 23.19 m ²
Garage	A: 49.34 m ²
Porch	A: 4.33 m ²
Upper Floor	A: 140.51 m ²
	P: 49.42 m
Balcony	A: 17.80 m ²
Total Area	436.28m²

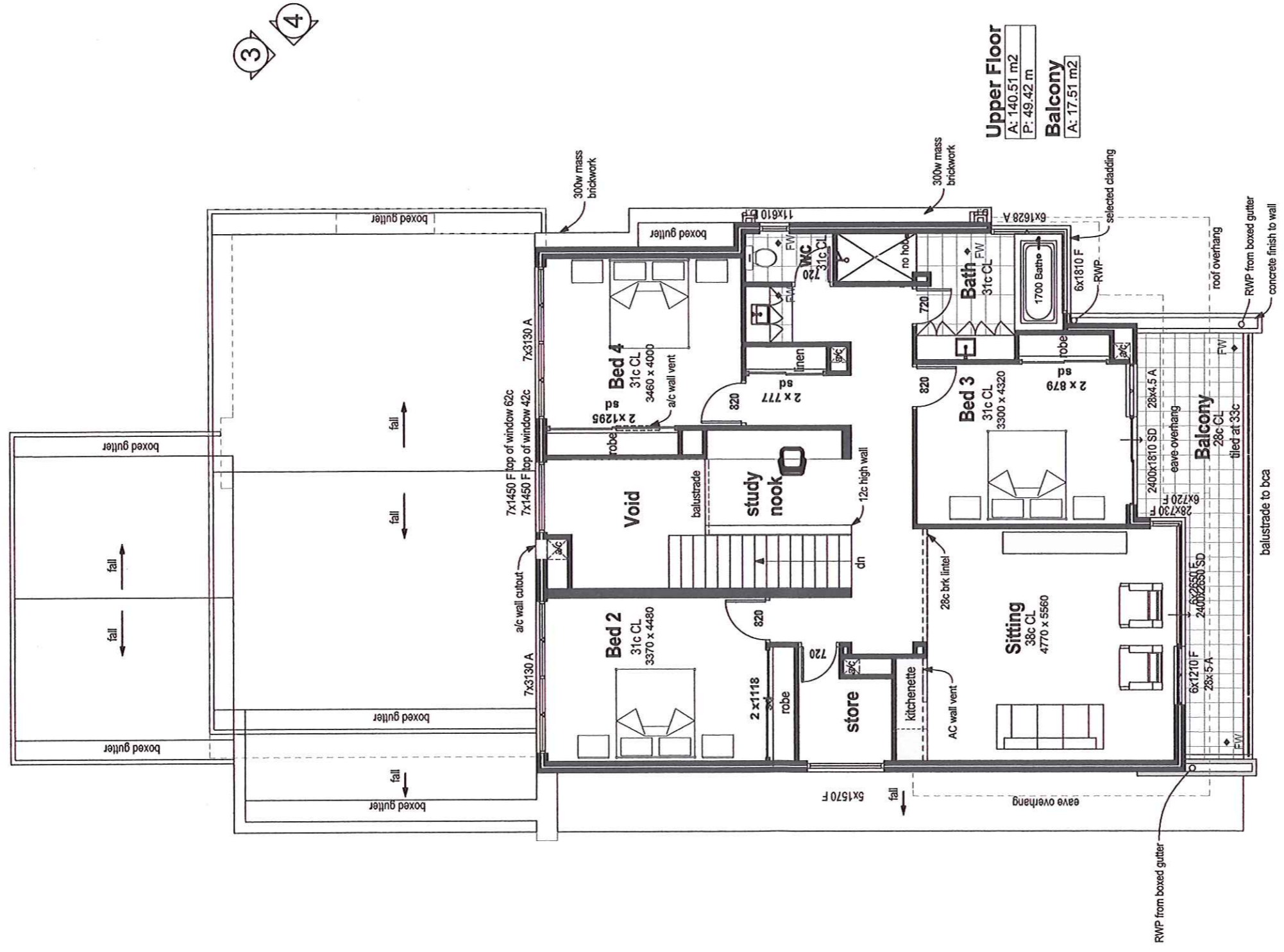
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rachel roberts
 residential designer

UPPER STOREY PLAN

DRAWN 20/7/2013

PROPOSED RESIDENCE FOR P & B CORNER
 #20 SWANSEA STREET, SWANBOURNE

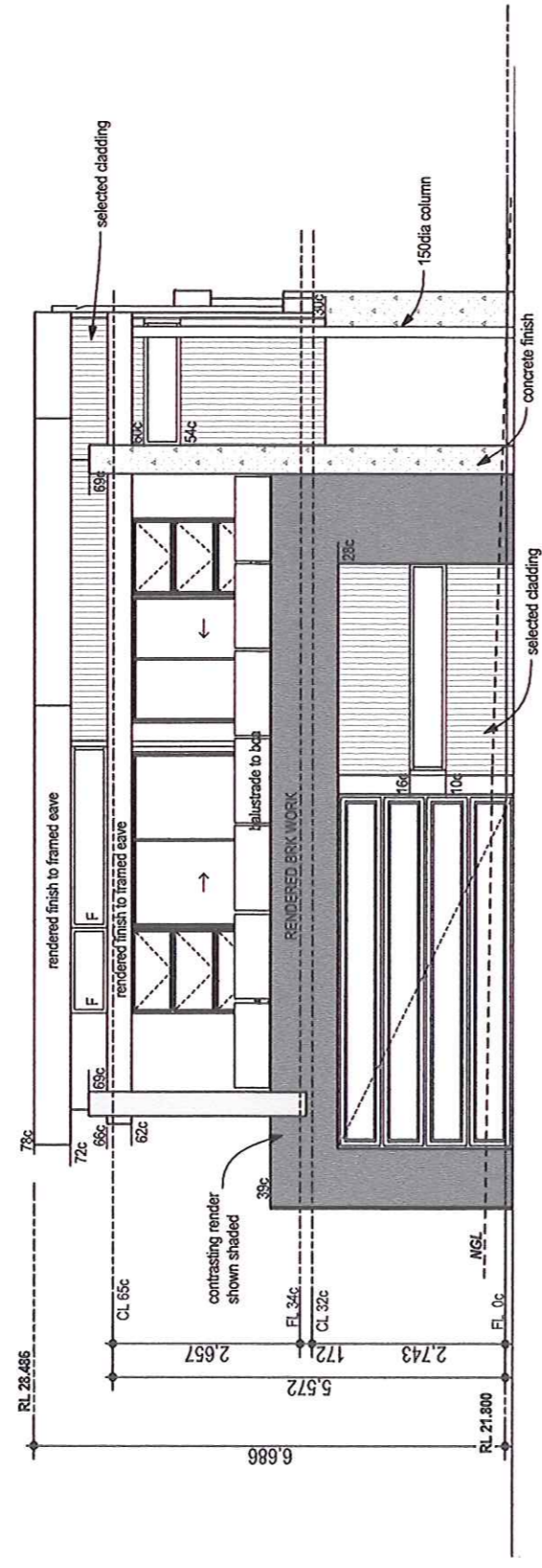


Upper Floor
 A: 140.51 m²
 P: 49.42 m

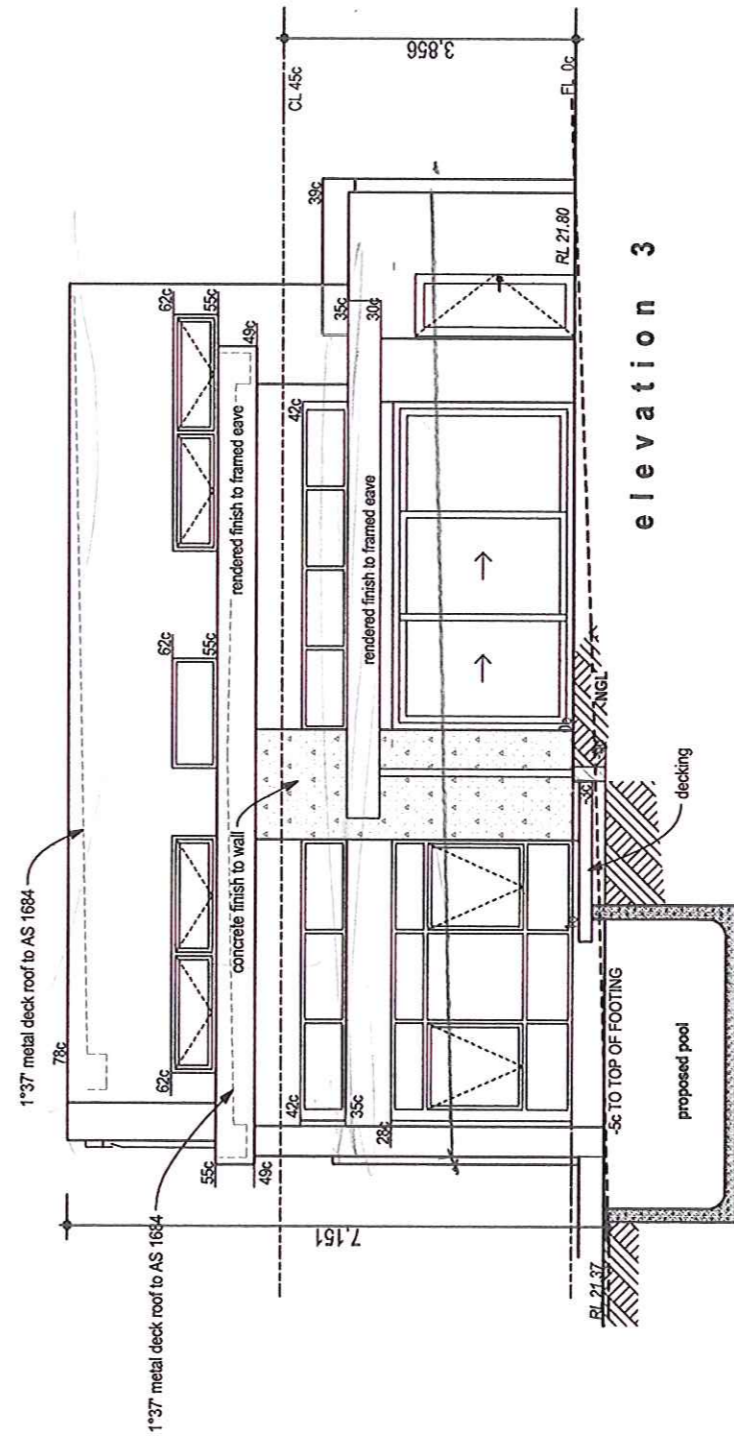
Balcony
 A: 17.51 m²

② ①

③ ④



elevation 1



elevation 3

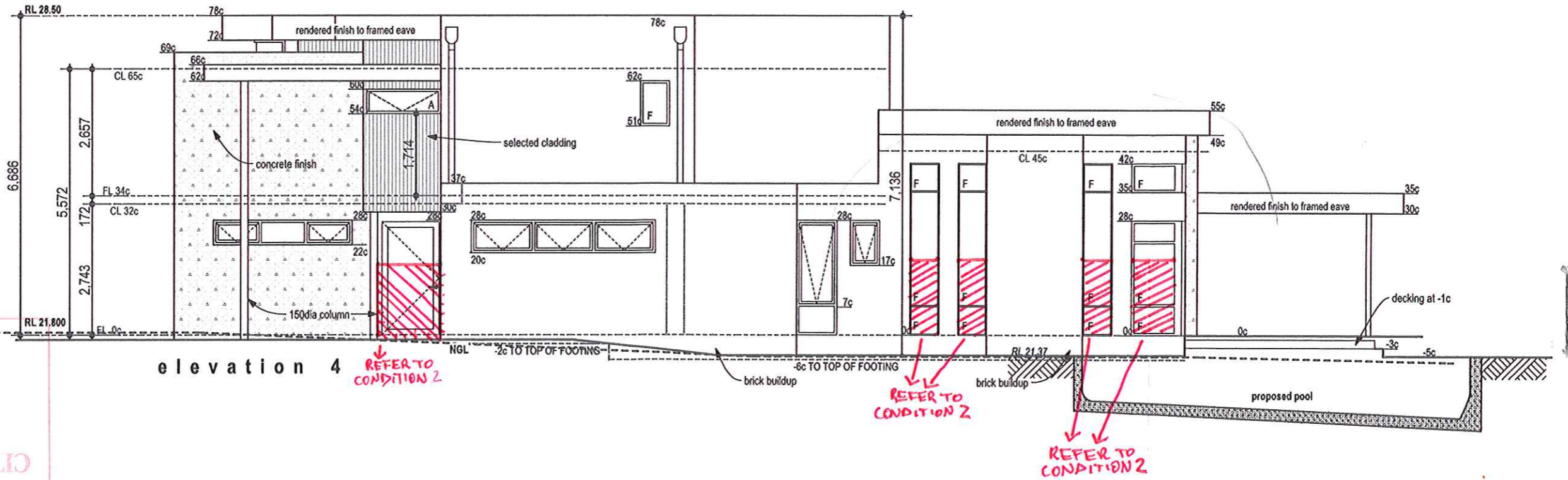
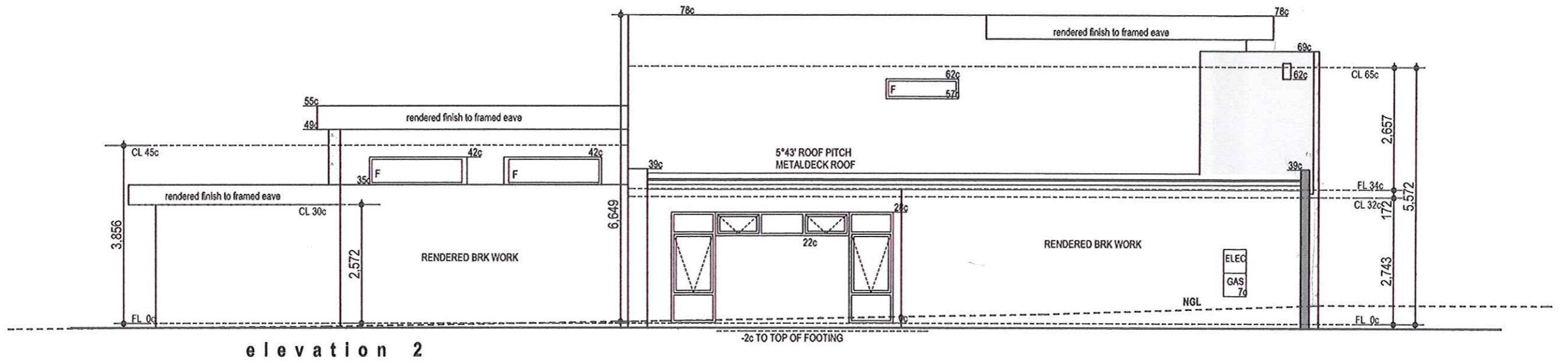
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PROPOSED RESIDENCE FOR P & B CORNER
 #20 SWANSEA STREET, SWANBOURNE

ELEVATIONS

DRAWN 20/7/2013

**PROPOSED RESIDENCE FOR P & B CORNER
#20 SWANSEA STREET, SWANBOURNE**



ELEVATIONS 2

DRAWN 20/7/2013

rachel roberts
residential designer

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CITY OF NEPLANDS

PROPOSED RESIDENCE FOR P&B CORNER

20 SWANSEA ST SWANBOURNE

TO : PLANNING DEPARTMENT.

REPLY TO NEIGHBOURING COMMENTS.

1. *The proposed primary street set back of 6.1m should be supported for the following reasons.*

- The reduction is in accordance with **Council Local Planning Policy Manual** clause 3.1 of Policy 6.18 which permits a reduced minimum setback in accordance with the provisions of the **Residential Design Codes** – Acceptable Development criteria, which allows for a minimum primary street set back of 6.0m in R15 zones.
- Nedlands Council is proposing in **Town Planning Scheme No3** to amend the current setback requirement in this area to align with the **Residential Design Codes** requirement for R15 zoning.
- The proposed reduction in setback is consistent with the existing setbacks in Swansea St with 50% of street frontages below 6m setbacks and a further 25% below 7m.
- With the properties in Swansea St orientated in a North – South direction, the affect on the morning light from a reduced set back would be negligible
- The majority of homes in Swansea St are relatively new homes with new construction currently taking place and further home developments in planning stages.
As the old homes that remain become the minority the collective appearance of the street is becoming one of a more modern beach style. However, houses can be diverse in age, shape and style yet combine to create a community identity.
- Creating a sympathetic building design to fit in with the streetscape does not mean the neighbouring house designs must be replicated. It implies being conscious of the areas natural enviroment density and style.
- The proposed development to 20 Swansea St is sympathetic to the existing streetscape.

2. *The western orientation of the front door will diminish privacy.*

- As discussed with the owners at 22 Swansea St the existing plans will be amended to show a screening panel to be fixed from the entrance wall through to the second 150mm steel post to create privacy and protect the entrance from any weather. This was not shown on the current plan as the type of screening method had yet to be confirmed.
- A raised garden bed will also be utilized behind the screen for further privacy with the use of shrubbery.

3. *The size of the structure in the rear setback is excessive and will affect adjoining neighbours entertaining areas.*

- The rear setback to the house was increased to 8m to assist in incorporating an Alfresco area to the rear. By doing this we considered it would reduce impact on rear neighbours by increasing the distance between the Alfresco and the rear fence.
- The affect on 18 Swansea St would be minimized due to the use of a 1.8m brick screen wall with louvered blades that could be closed or opened as required on the left hand side of the alfresco area.
- As the Alfresco is offset to the left of the house it was considered this would reduce the impact on the right hand neighbours.
- The alternative to this proposal would be to delete the Alfresco area as a structure and move the rear of the house back to a 6m setback and incorporate an entertaining area within that remaining space. I think this would have a far greater impact on the adjoining neighbours entertaining areas.
- Although the Alfresco is an open sided structure on three sides I think clever use of screening with fabrics and roll down blinds ect will assist in increasing privacy.

4. *The two storey height of the building will block sunlight to adjoining buildings.*

- Both buildings on either side of 20 Swansea St are of a two storey construction.
As the proposed maximum roof height at 20 Swansea St is 7m, it would place it approximately 1.5m below the roof height of 22 Swansea St.

5. *The western facing windows in the back living area diminish privacy.*

- As discussed with the owners at 22 Swansea St and suggested by Thomas Geddes, the windows will be glazed with obscured glass or a permanent screen to design codes requirements erected to maintain privacy.
- We would seek a ruling from council regarding the requirement for screening from 22 Swansea St from their rear deck under *State Planning Policy 3.1 R.D.Codes part 5.4.1 / C1.1*.

6. *Pool pump noise will negatively affect adjoining neighbours.*
 - The pool pump will be housed in a suitable cover and comply with all Environmental Health Regulations.

7. *The Upper windows of the ground floor will be seen from adjoining rear neighbours. Privacy screen to be erected.*
 - As all ground floor windows from adjoining neighbours are visible from the rear of 20 Swansea St we shall choose to ignore this request. However if a rear neighbour would like to erect a screen to enhance privacy between properties we would be more than happy to discuss the possibilities.

YELLOW LINE SHOWS A 6.1m FRONT SETBACK IN RELATION TO EXISTING HOMES SETBACKS





VIEW TO WEST NEIGHBOUR / 22 SWANSEA ST



VIEW OF ELEVATED REAR DECK AT 22 SWANSEA ST WHICH CREATES A DIMINISHED PRIVACY TO THE REAR OF 20 SWANSEA ST



VIEW TO REAR NEIGHBOUR



VIEW TO EAST NEIGHBOUR / 18 SWANSEA ST

PD44.13 – Attachment 4 – Site Photographs



No. 20 (Lot 6) Swansea Street, Swanbourne

PD44.13 – Attachment 4 – Site Photographs



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No. 20 (Lot 6) Swansea Street, Swanbourne

PD44.13 – Attachment 4 – Site Photographs



No. 20 (Lot 6) Swansea Street, Swanbourne

No. 94 (Lot 1667) Birdwood Pde, Dalkeith

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CHECK ALL DIMENSIONS ON SITE BEFORE COMMENCING WITH THE WORK SHOWN ON THIS DRAWING. WORK TO FIGURED DIMENSIONS ONLY. DO NOT SCALE. DRAWINGS REPORT ANY DISCREPANCIES TO THE METAL GROUP FOR A DECISION BEFORE PROCEEDING WITH WORK.



REVISION	DESCRIPTION	DATE
01	ISSUED TO BMV	26.09.12
02	AMENDED EXTERNAL WORKS	27.09.12
03	AMENDED CAR PARKING	02.10.12
04	ISSUED TO BMV	18.04.13



SUNSET BUILDING Q
PROJECT ADDRESS
80 BIRDWOOD PARADE,
DALKEITH, WA, 6009

DRAWING TITLE
SITE PLAN

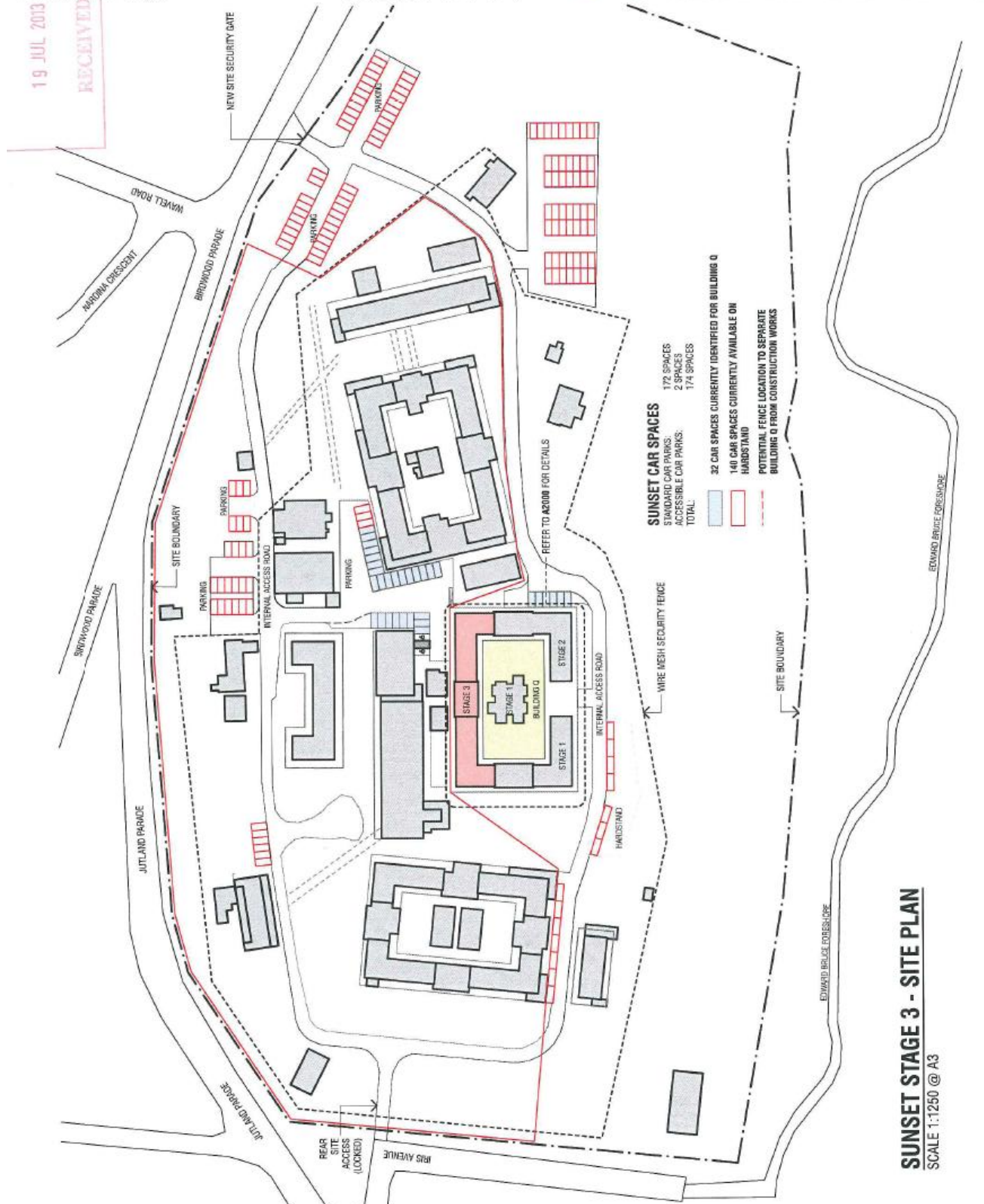
SCALE
1:1250 @ A3

DATE
18/04/13

DRAWING NO.
A1000

REVISION
04

STATUS
PLANNING



SUNSET STAGE 3 - SITE PLAN
SCALE 1:1250 @ A3

No. 94 (Lot 1667) Birdwood Pde, Dalkeith

PLEASE NOTE: CHECK ALL DIMENSIONS ON SITE BEFORE COMMENCING WORK. DIMENSIONS ON THIS DRAWING TO BE USED FOR PERMITS ONLY. DO NOT SCALE. ADVANCE REPORT ANY DISCREPANCIES TO THE METAL GROUP FOR A DECISION BEFORE PROCEEDING WITH WORK.

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EXISTING HARDSTAND

EXISTING 1971 STRUCTURE TO REMAIN

EXISTING GLASS HOUSE TO REMAIN

PASTING LANDSCAPE

ON ORIGINAL @ A3

0 1 2 3 4 5

SUNSET STAGE FLOOR AREAS

EXISTING STAGE 1 (WEST WING / MEETING ROOM): 444.6 sqm
 EXISTING STAGE 2 (EAST WING): 554.6 sqm
 PROPOSED STAGE 3 (NORTH WING): 489 sqm
 TOTAL FLOOR AREA BUILDING Q: 1288.2 sqm

REVISION	DESCRIPTION	DATE
01	ISSUED TO BMW	28.05.12
02	AMENDED EXTERNAL WORKS	27.05.12
03	AMENDED CAR PARKING	02.10.12
04	ISSUED TO BMW	18.04.13



NORTH POINT

SUNSET BUILDING Q

PROJECT ADDRESS
 80 BIRDWOOD PARADE,
 DALKEITH, WA, 6009

DRAWING TITLE

STAGE 3 - FLOOR PLAN

SCALE
 1:200 @ A3

DATE
 18/04/13

REVISION

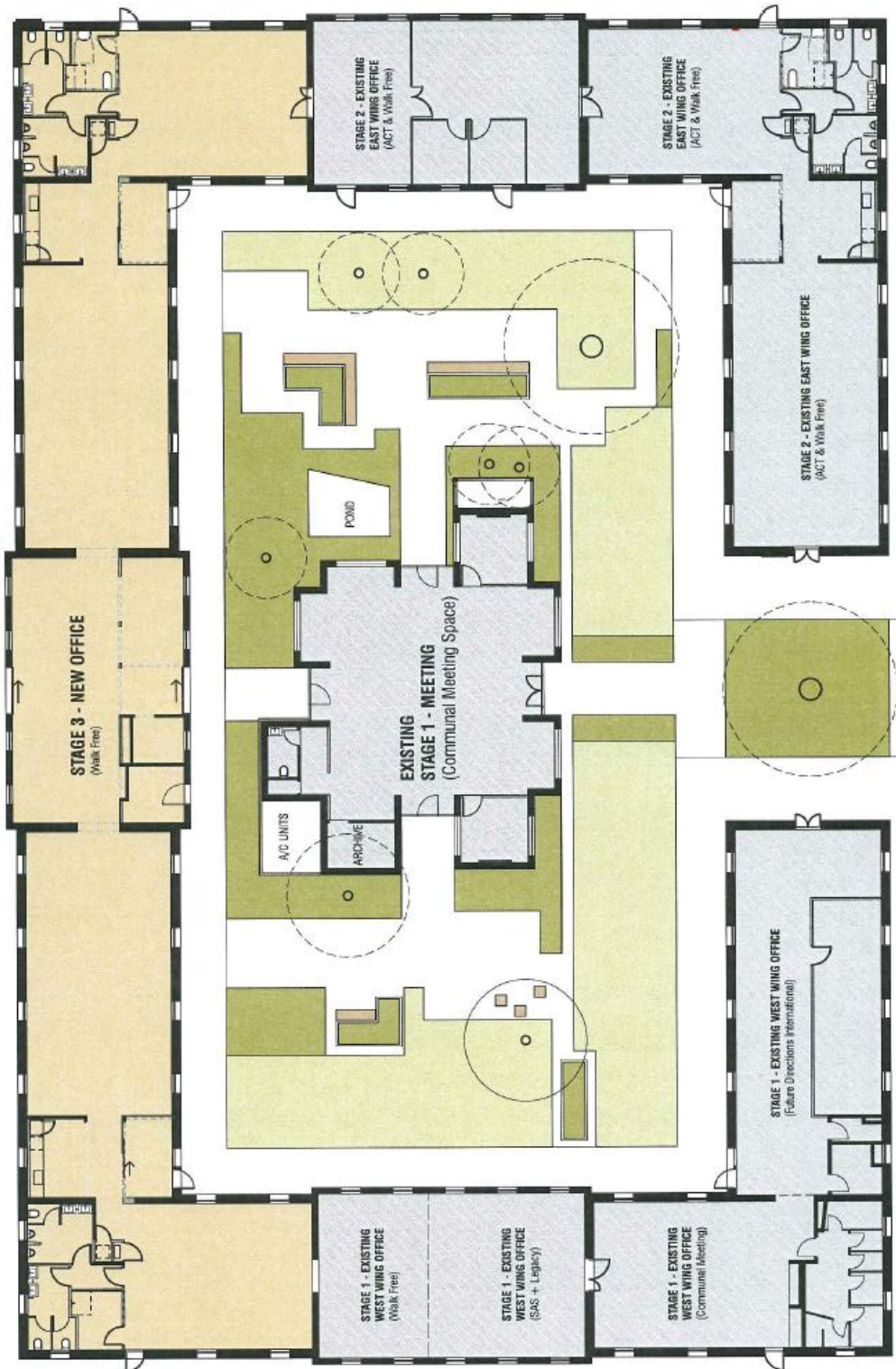
04

DRAWING NO

A2000

STATUS

PLANNING



SUNSET STAGE 3 - FLOOR PLAN SCALE 1:200 @ A3

No. 94 (Lot 1667) Birdwood Pde, Dalkeith



Building Q – Southwest



No. 94 (Lot 1667) Birdwood Pde, Dalkeith
Entry & Egress Point – Birdwood Parade



Current Condition of Floorspace – Proposed (Stage 3)



No. 94 (Lot 1667) Birdwood Pde, Dalkeith
Current Condition of Floorspace – Previous Approval (Stage 2)



Existing Car-Parking – Building Q

No. 94 (Lot 1667) Birdwood Pde, Dalkeith



Unmarked, Onsite Car-Parking Bays



Physical Obstruction to Car-Parking Bays – Locked Gate

No. 94 (Lot 1667) Birdwood Pde, Dalkeith



Physical Obstruction to Car-Parking Bays – Unserviceable Condition

No. 94 (Lot 1667) Birdwood Pde, Dalkeith



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18 April 2013

Mr Mike Cole
A/Chief Executive Officer
City of Nedlands
71 Stirling Highway
6009 WA

ATTENTION: Ms Jennifer Hayes

Dear Mr Cole

Re: Planning Approval for Stage 3 of Q Block at Sunset Hospital site, Lot 9547 Birdwood Parade for office use.

Summary

I refer to the above correspondence where approval was given by the WAPC to develop Stage 2 of Q Block. This approval was for "...stage 2 (building Q) (inclusive of the additional 91 sqm) as shown on application on Drawing No. A003 Rev 01 date stamped 14 June 2012" (Attachment 1).

I advise that the Australian Children's Trust has now determined to relocate the administrative operations of Walk Free www.walkfree.org from where it is currently based in the USA to Perth and our offices at Sunset. Walk Free is a philanthropically funded, not for profit organisation, which was started by the Australian Children's Trust and is funded wholly by the Forrest Family Trust.

The Western Australian Government - via Building Management and Works (BMW) - has now agreed to the continued use of Q Block and the ancillary facilities by the ACT - and the ACT now has a signed 2 year lease with a 2 year extension option from the WA Government - giving a total forward lease period of 4 years until 2016.

The current application is for approval to re-furbish and use the section shown on the attached plans A1000 and A2000 as "Stage 3" for office use.

Attached plan A2000 also shows what is already approved as "Stage 1" and "Stage 2". Stage 1 was approved in 2010 and Stage 2 received Approval to Commence Development on the 25th July 2012.

80 Birdwood Parade • Dalkeith • WA 6009 • Tel: 08 6460 4949 • Fax: 08 9386 1245 •
Post Office Box 3155, Broadway Nedlands, WA 6009
www.australianchildrenstrust.org.au • www.fiftythousandjobs.com.au

No. 94 (Lot 1667) Birdwood Pde, Dalkeith



The additional floor space in Stage 3 is 499 Sq m. thus expanding the total floor area in use in Q Block from 739 Sq m. to 1238 Sq m.

We currently envisage Stage 3 will be used to accommodate up to 30 additional staff taking total numbers to be working in the building to around 60.

Additional Traffic

There are currently around 30 people working at Sunset in Q-Block – although on any given day there are staff travelling or out of the office so they are rarely all in attendance at the same time.

As stated above Stage 3 will provide capacity for an additional 30 staff.

Based on each of these extra 30 people making a trip to and from work; 1 in 5 of them making an extra trip for an outside appointment during the day, and (say) allowing for an extra 20 visitors/couriers each day, Stage 3 will potentially generate an additional 56 vehicular total movements into the site each day. This equates to a total of 112 traffic movements on Birdwood Parade - when these vehicles' arrival and departure are counted.

The most recent available traffic count was taken in 2006 on Birdwood Parade between Throssell Street and Sadlier Street. This showed that the average daily (weekday) number of vehicles was 2041.

As this figure is somewhat dated it is necessary to consider what it might be today. The most recent count before that was 1804 vehicles per day taken in 2003. Thus over that 3 year period the number of vehicles grew by a total of 237 per day – or approximately 13%. If we simply apply this growth rate to the two 3 year periods since 2006 we can extrapolate that the current vehicle numbers are in the vicinity of 2600 vehicles per day.

Obviously this is an extrapolation of historical data however it is probably a conservative figure given the extra development that has taken place in the western suburbs and the new commercial nodes in Claremont and Cottesloe. Further the increased loads on Stirling Highway are prompting many drivers to seek alternative routes that bypass the Highway and Birdwood Parade is one of these.

It is reasonable to conclude that the proposed Stage 3 will not have a material impact on traffic movements on Birdwood Parade.

No. 94 (Lot 1667) Birdwood Pde, Dalkeith



Consultation with surrounding landowners

In an earlier approval (Your ref 08-50122-1 dated 22nd June 2012) we were told that The City of Nedlands advised that: -

"in respect of Condition No 2, the applicant is advised that Stage 3 would result in an increase in traffic that would require consultation with surrounding landowners and as such further planning approval is required prior to Stage 3 being undertaken"

In response to this, and despite the very low impact on current traffic numbers of Stage 3 we agreed with the City of Nedlands on an appropriate Consultation Strategy with relevant affected landowners and Councillors.

The City believed that the only residents the completion of Stage 3 may impact on are those reasonably close to the gate as a result of increased traffic movement. This includes 10 properties whose owners we agreed with the City should be consulted.

Our consultation strategy included inviting all these landowners (or their representatives) along with the Mayor, all City of Nedlands Councillors, senior City of Nedlands staff and senior relevant personnel from Building Management and Works to a site visit and full presentation on the ACT's plans for Stage 3.

This invitation was for Monday the 24th of September 2012 at 5.00 pm. All landowners were invited by mail with telephone follow up to those whose number we could identify.

No residents expressed any interest in attending the briefing in fact only the Mayor and one other Councillor were interested. Accordingly the briefing was cancelled and the Mayor and Councillor Argyle were each provided a private briefing.

The ACT believes this is a clear indication that the ACT's activities to date have had no negative impact on local residents and that indeed they are not concerned about an expansion of ACT's activities into the rest of Q-Block.

Development of Stage 3

The Stage 3 development will be undertaken to the equivalent standard and with the same consideration to heritage issues as per Stage 2. Floor plan layouts, finish and fit-out will effectively be the same as Stage 2.

The externals of the building will be refurbished to a reasonable condition commensurate with the term of the lease and the roof will be repaired to ensure it is waterproof for the same period.

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Heritage

Our plans have been discussed with the Heritage Council of WA at a meeting on site where the Executive Director and the Manager Development Referrals also viewed the works done on Stage 2.

They were complimentary of the works done and comfortable with the plans presented. As a result of the visit we have agreed to have a firm of Heritage Architects review the renovation and building fit-out plans before they are submitted.

On Site Car Parking

Within Stages 1 and 2, the Australian Children's Trust and the other "not for profit" organisations that have been approved to sub-lease have a total of around 30 staff. For the current staff there are currently 32 designated Car Parking Bays. For the Stage 3 works it is estimated that 30 additional bays will be required.

The new Stage 3 office staff will require the use of 30 additional parking bays. From this, the ACT have identified a total of 172 car parking bays on existing hardstand within the Sunset precinct – including 2 Accessible Bays for people with disabilities. This exceeds requirements and will allow ample provision for film production and other uses of the site.

I trust this application along with the accompanying forms and attachments meets your requirements. Should you have any further queries please do not hesitate to contact Tim Davoren on 0403 547 723 or tim.davoren@themetagroup.com.au.

Yours Sincerely

Grant Vernon
Chief Operating Officer
Australian Children's Trust



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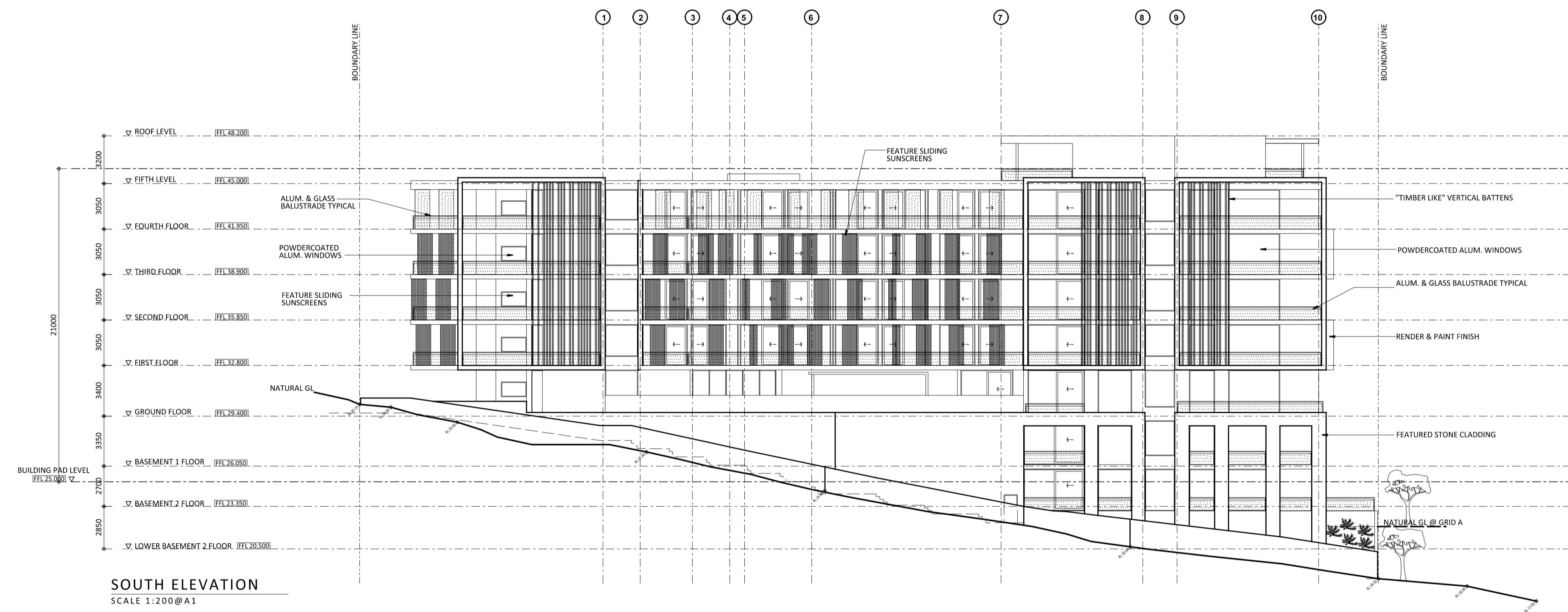
**Attachment 1 - Locality Plan
No. 2 Milyarm Rise, Swanbourne**

03 October 2013

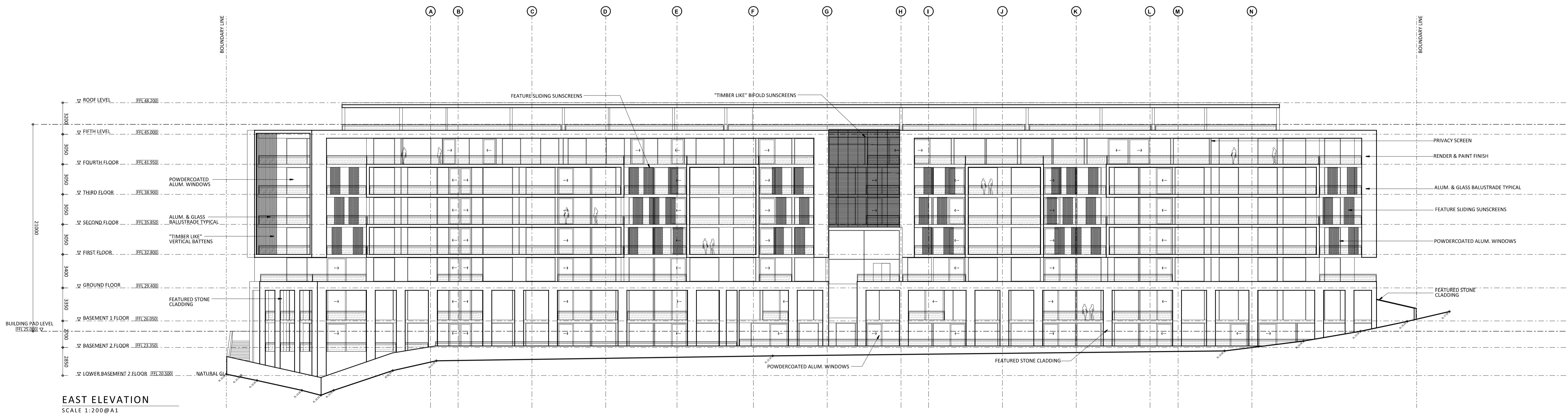


Attachment 2 - Development Plans

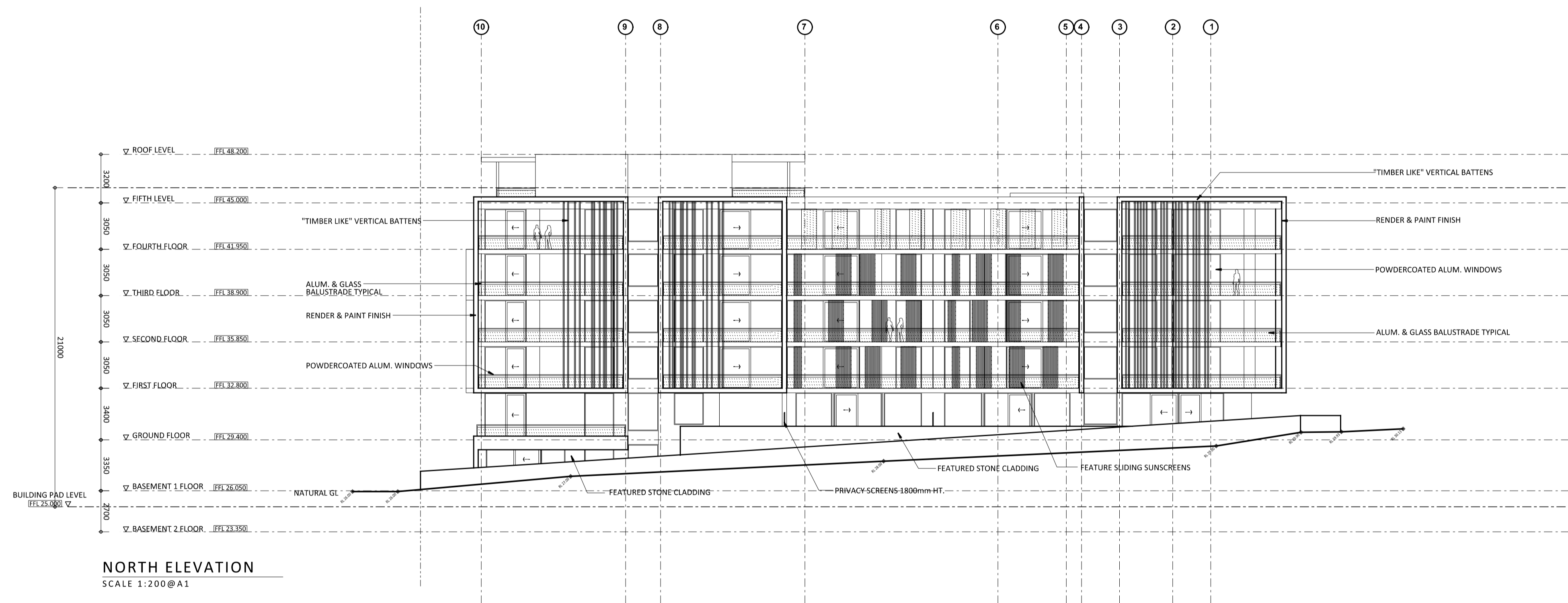
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Attachment 2 - Development Plans

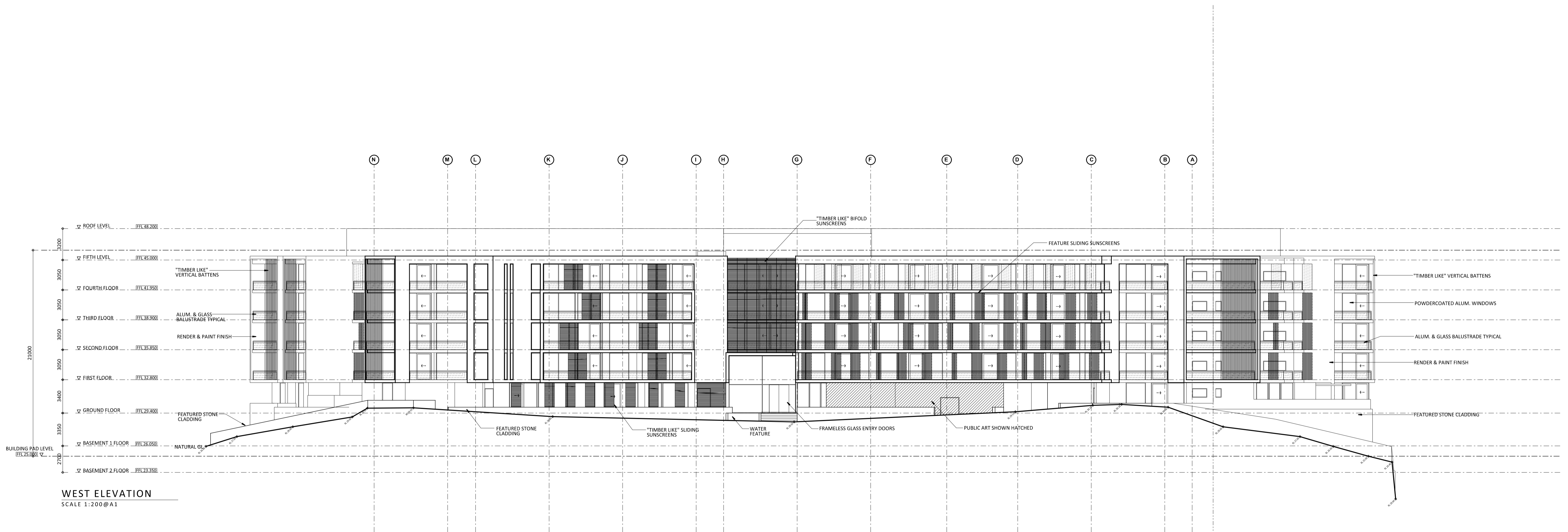


Attachment 2 - Development Plans



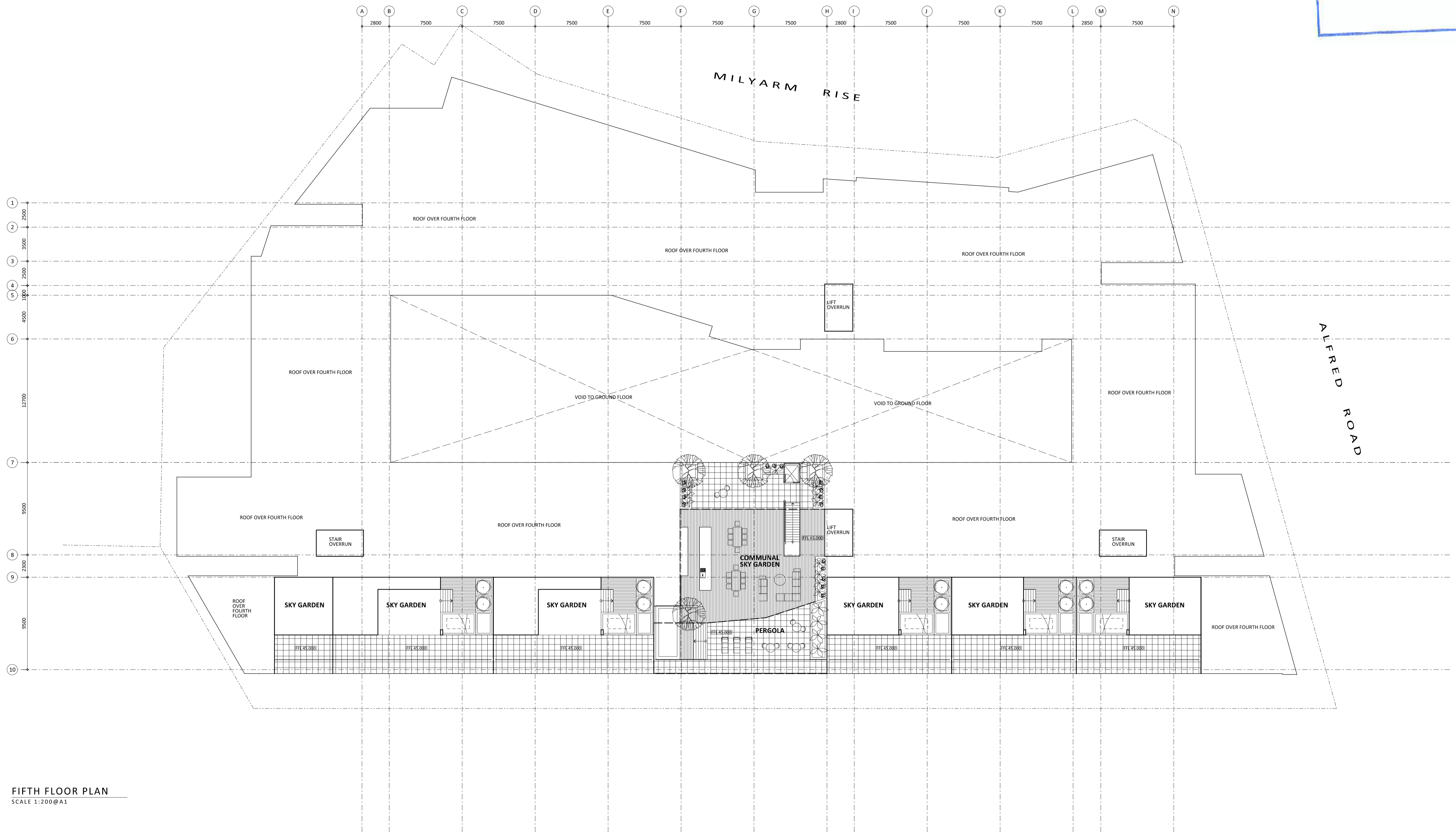
Attachment 2 - Development Plans

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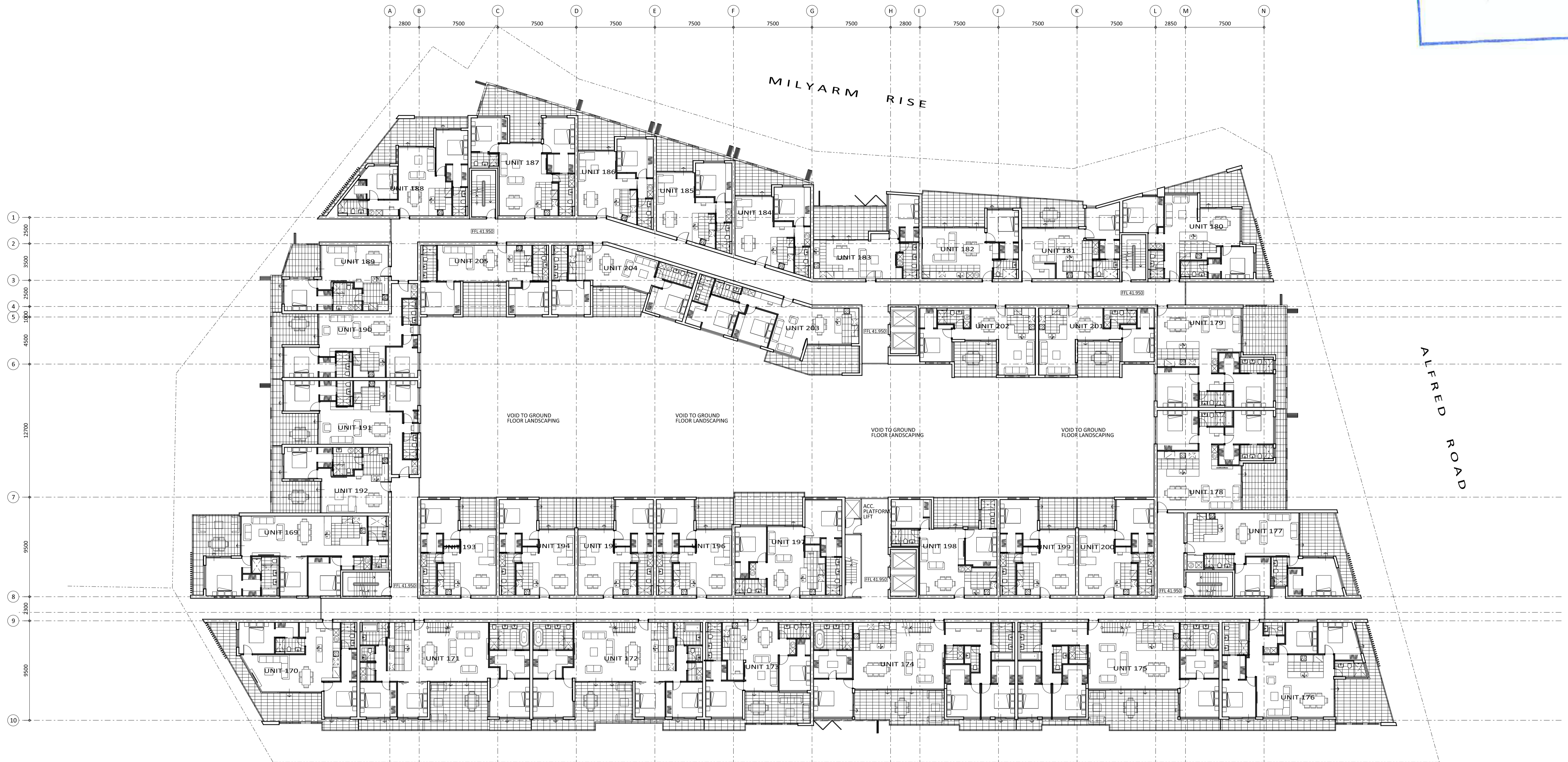
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27 SEP 2013



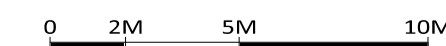
FIFTH FLOOR PLAN
SCALE 1:200@A1

Attachment 2 - Development Plans

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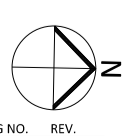


FOURTH FLOOR PLAN
SCALE 1:201@A1



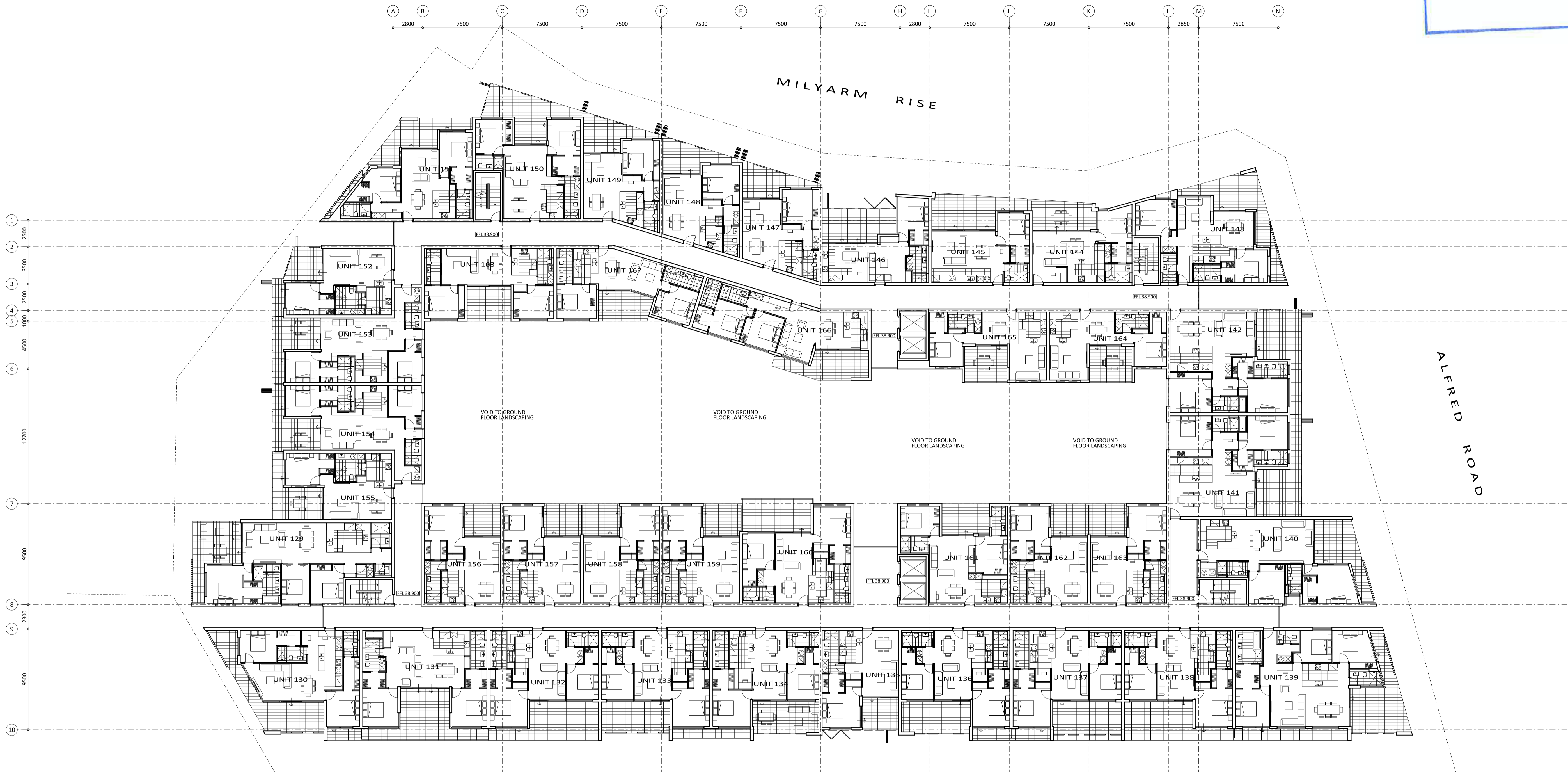
HILLAM ARCHITECTS
architecture urban planning interior design

DATE	REV	NOTE
22.08.13	C	ISSUED TO CITY OF NEOLANDS
21.08.13	B	ISSUED TO CITY OF NEOLANDS
24.06.13	A	ISSUED TO PLANNER



Attachment 2 - Development Plans

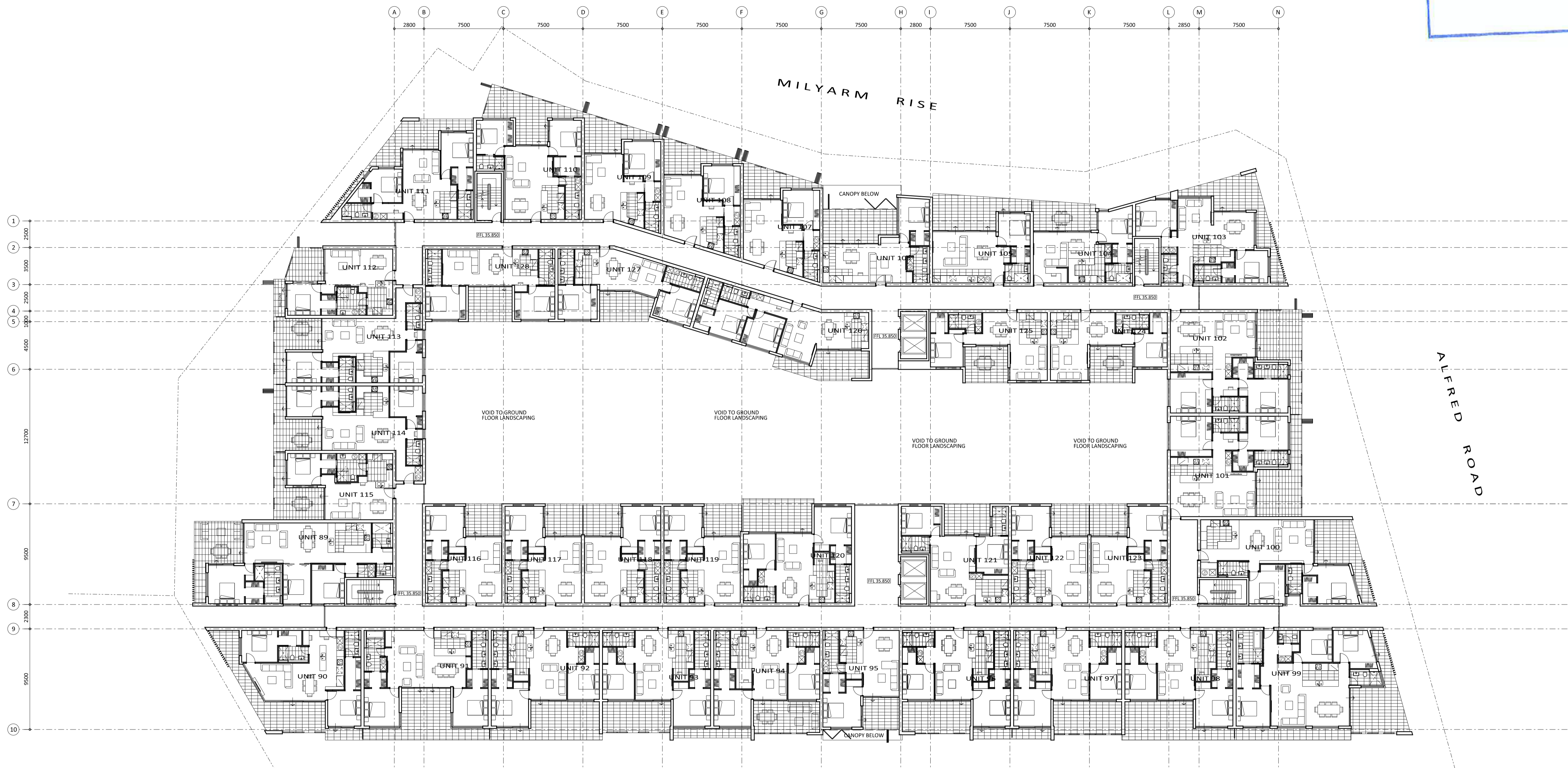
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27 SEP 2013



THIRD FLOOR PLAN
SCALE 1:200@A1

Attachment 2 - Development Plans

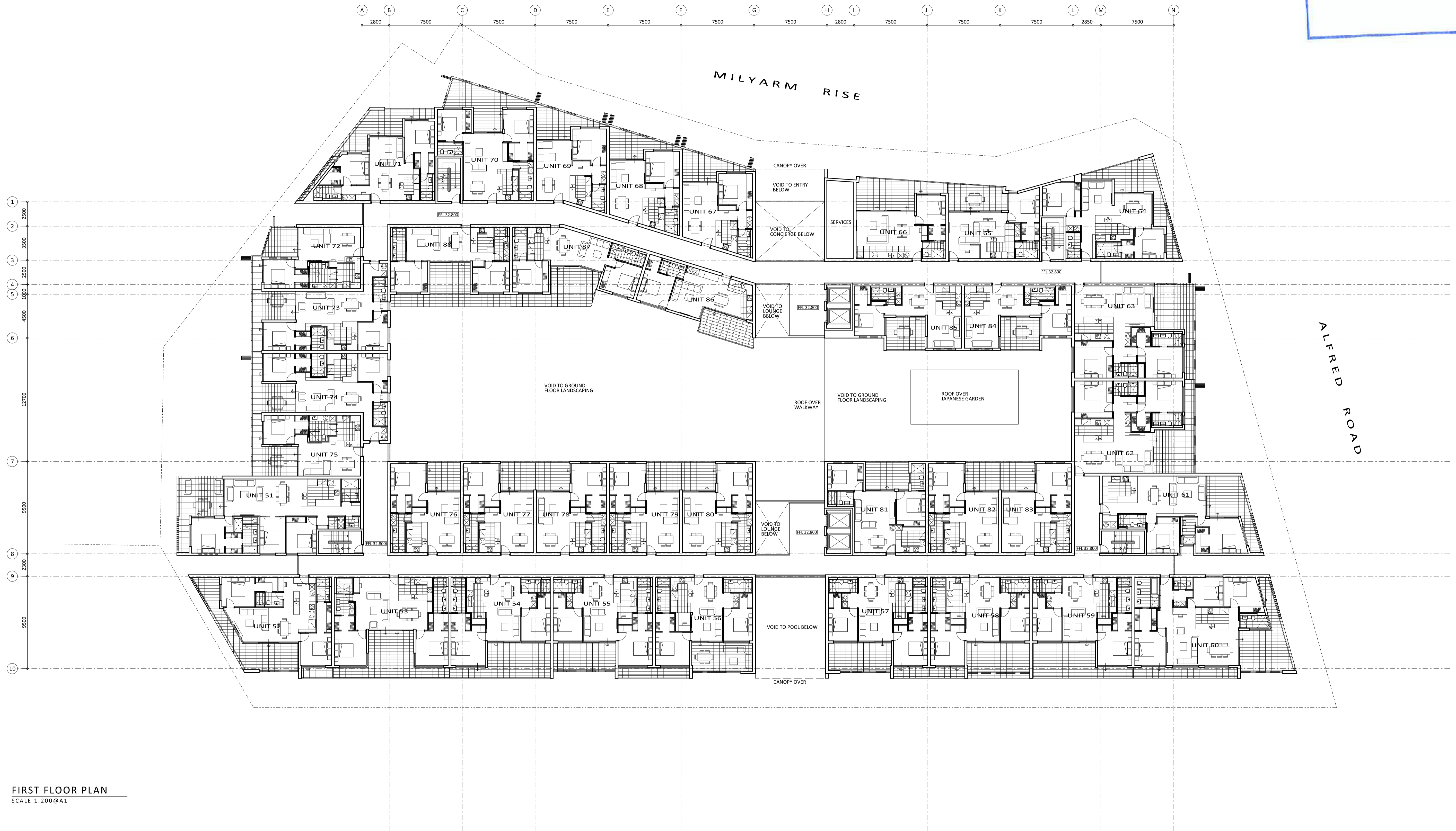
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SECOND FLOOR PLAN
SCALE 1:200@A1

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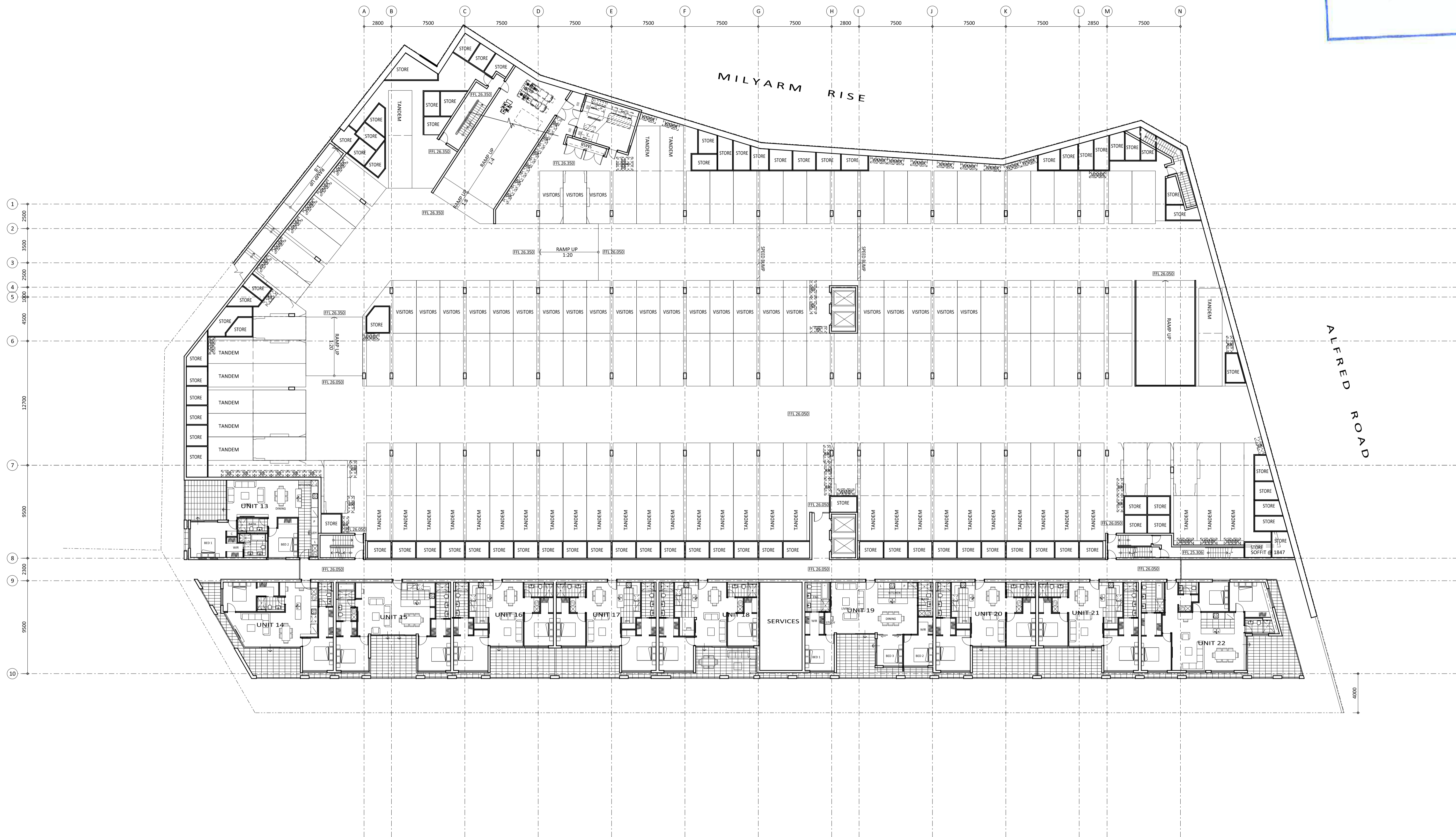
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GROUND FLOOR PLAN
SCALE 1:200@A1
12 STORES FOR GROUND
24 nos. 1100L BINS

Attachment 2 - Development Plans

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RECEIVED
27 SEP 2013

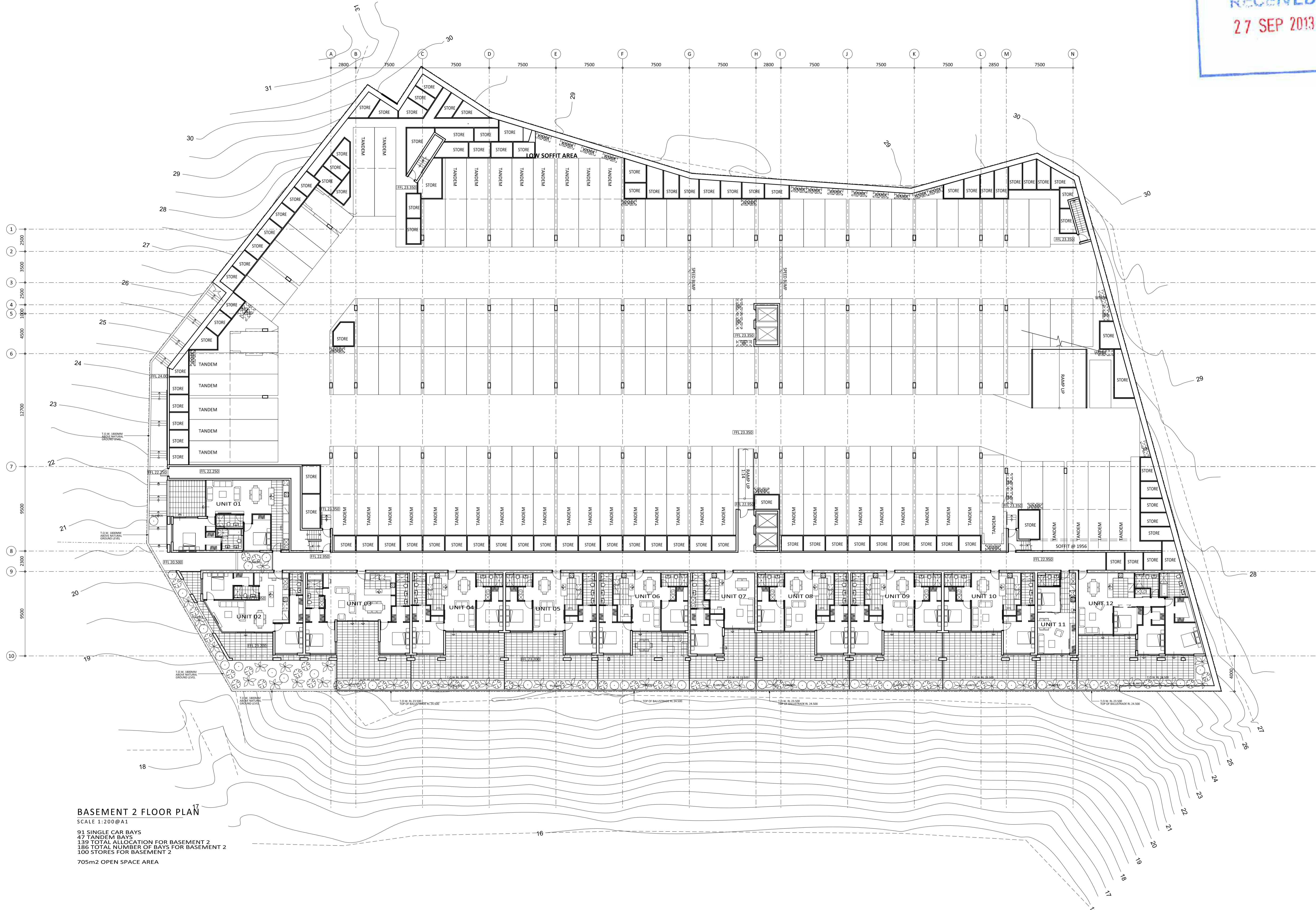


BASEMENT 1 FLOOR PLAN
SCALE 1:200 @ A1

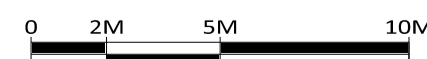
93 SINGLE CAR BAYS
40 TANDEM BAYS
133 TOTAL ALLOCATION FOR BASEMENT 1
173 TOTAL NUMBER OF BAYS FOR BASEMENT 1
82 STORES FOR BASEMENT 1

Attachment 2 - Development Plans

CITY OF NEEDLANDS
RECEIVED
27 SEP 2013

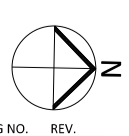


BASEMENT 2 FLOOR PLAN
SCALE 1:200 @ A1
91 SINGLE CAR BAYS
47 TANDEM BAYS
139 TOTAL ALLOCATION FOR BASEMENT 2
136 TOTAL NUMBER OF BAYS FOR BASEMENT 2
100 STORES FOR BASEMENT 2
705m² OPEN SPACE AREA



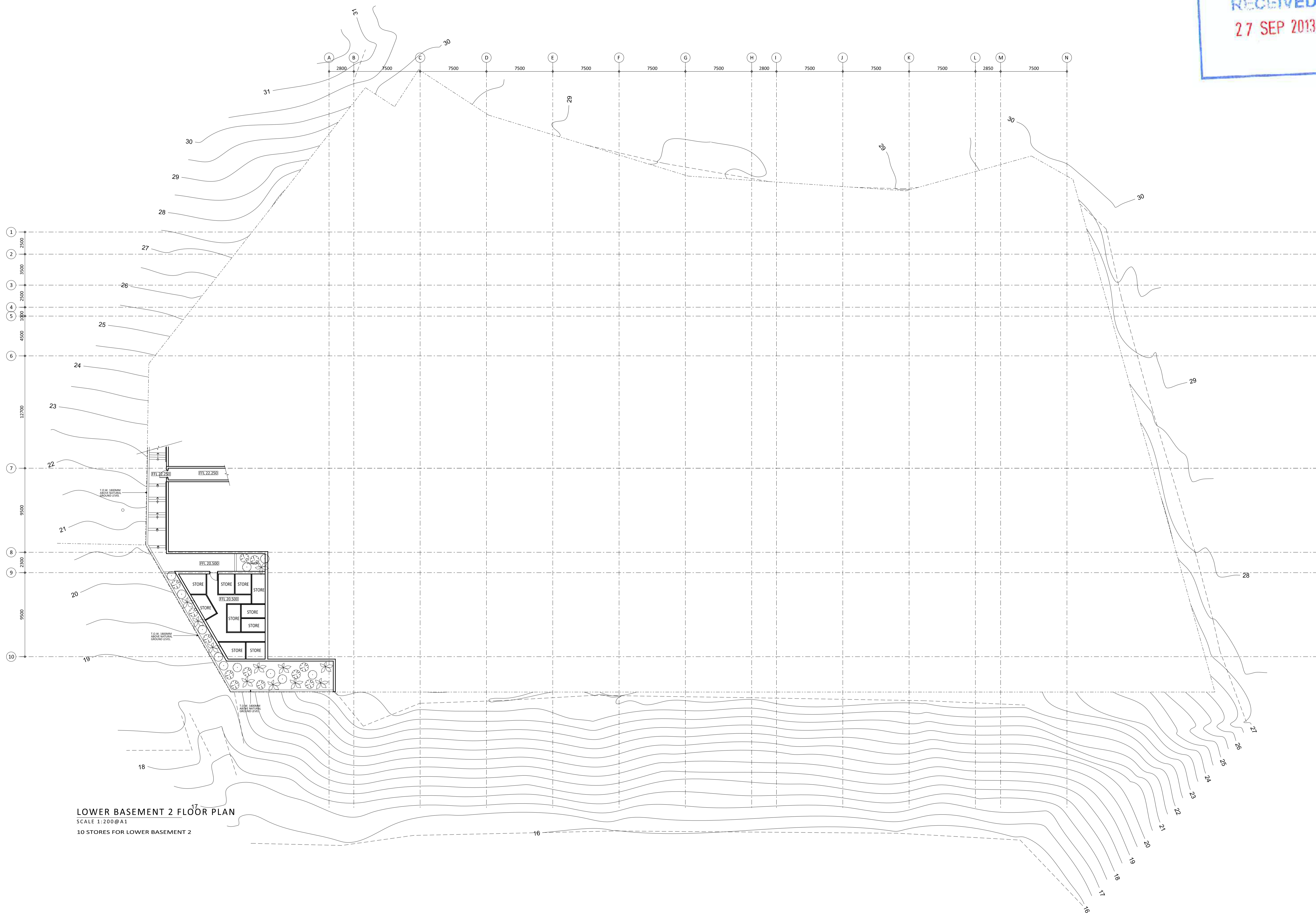
HILLAM ARCHITECTS
architecture urban planning interior design

DATE	REV	NOTE
27.09.13	E	ISSUED TO CITY OF NEEDLANDS
21.09.13	D	400 OVERALL SHOP IN BUILDING HEIGHT
21.09.13	C	ISSUED TO CITY OF NEEDLANDS
20.09.13	B	ISSUED TO BPW
24.08.13	A	ISSUED TO PLANNERS



Attachment 2 - Development Plans

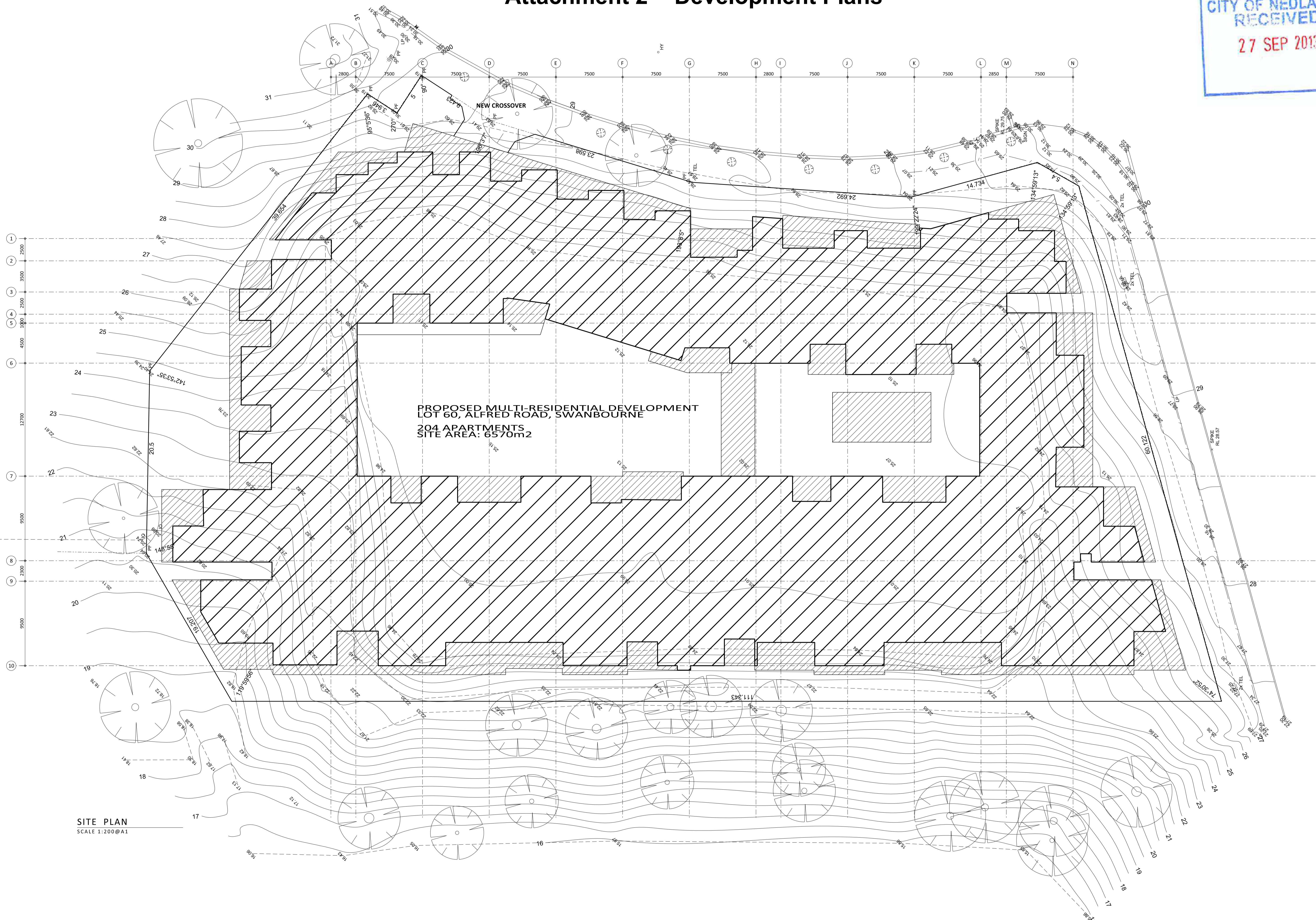
CITY OF NEDLANDS
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LOWER BASEMENT 2 FLOOR PLAN
SCALE 1:200 @ A1
10 STORES FOR LOWER BASEMENT 2

Attachment 2 - Development Plans

CITY OF NEGLANDS
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27 SEP 2013



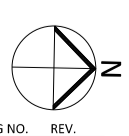
PROPOSED MULTI-RESIDENTIAL DEVELOPMENT
LOT 60, ALFRED ROAD, SWANBOURNE
204 APARTMENTS
SITE AREA: 6570m²

SITE PLAN
SCALE 1:200@A1

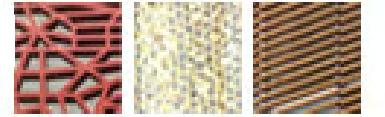
0 2M 5M 10M

HILLAM ARCHITECTS
architecture urban planning interior design

27.09.13	B	ISSUED TO CITY OF NEGLANDS
24.07.13	A	ISSUED TO CITY OF NEGLANDS
DATE	REV	NOTE



A1-01 B



DEVELOPMENT APPLICATION
LOT 60 | ALFRED RD, SWANBOURNE
- JULY 2013 -



MULTI RESIDENTIAL DEVELOPMENT
LOT 60 | ALFRED RD | SWANBOURNE



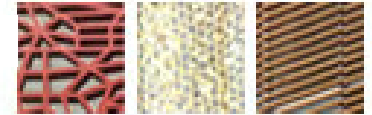


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END OF DOCUMENT

Record of issue

Issue	Status	Date	Revision
Issue to City of Nedlands	DA SUBMISSION	18/07/2013	-



1.0 Introduction

This report forms the Development Application for a proposed commercial and residential development within Swanbourne and refers to the following documents:

- SWANBOURNE DESIGN GUIDELINES B

1.1 Site Information

Address:

- Lot 60, Alfred Road, Swanbourne

Developer: Alfred Rd Developments Pty Ltd

Architect: Hillam Architects

Planning Data:

- **Site Area:** 6552m²
- **R-Code:** R80
- **Plot Ratio:** No Plot Ratio Applicable
- **North, East, West Setbacks:** 4m
- **South Setbacks:** 2m
- **Building Height:** 21.0 metres & 5 storeys to the street front
- **Access & Service:** Milyarm Rise
- **Finished Floor Levels:** Refer to drawings
- **Sewer Easement:** N/A

Planning Authority: City of Nedlands

Local Council: City of Nedlands





1.2 Design Summary

The development of Lot 60 Alfred Road delivers diverse housing options to those wanting to live in the precinct formally occupied by the Swanbourne High School. The project will complement the mix of larger houses, small lot houses, townhouses and small apartment projects.

The site has been carefully planned for this type of development as the bulk and scale of the project will have virtually no direct impact on surrounding housing. The project is ideally located to bring more people to a location which is eminently convenient to local workplaces, recreation spaces and amenities.

The development consists of a diverse range of studio, one, two and three bedroom dwellings. Dwelling sizes vary considerably both within each type and across the variety of living options. The diverse mix of apartments reflects the fact that research shows that this location is ideal in offering convenient accommodation to many who will at different stages of their lives. This will include but not be limited to local “Baby Boomers” looking for low maintenance lock up and leave accommodation; young professionals working close by and young adults being supported by parents into their first homes.

Two hundred and five dwellings are provided with car parking facilities which exceed the requirements of the design guidelines and residential codes.

Significant points of interest are :

1. Design Quality

The vision for the project is to deliver premium quality apartments well to suit to the vision of the overall precinct. The amenity within the project will exceed virtually all other apartment projects in Perth. The highly articulated building form coupled with a diverse range of materials will be a positive addition to the streetscape and views from adjacent areas.

2. Sustainability

The development will significantly exceed the requirements of the design guidelines by achieving a minimum of five stars Nationwide House Energy Rating Scheme (NatHERS) rating. Various progressive systems will be employed to ensure appropriate and practical sustainable outcomes will be provided.



3. Compliance

The development fits with the bulk and scale requirements of the Swanbourne Design Guidelines.

The guidelines envisage five stories of apartments with a loft or sky garden. We propose not to locate the sky gardens on the street side of the project. This will diminish the scale of the project when viewed from the dwellings within the Swanbourne High School precinct. The guidelines suggest one level of development below the ground floor, however diagrams in the guidelines do not account for the amount of fall across the site. Two levels of below street level construction can easily be accommodated, allowing dwellings at the lowest level to come out at the natural ground level.

4. Street Level Activation

The concept orientates apartments to address all faces. Apartments facing north, activate the Alfred Road frontage. West facing apartments activate Milyarm Rise. At ground level north of the entry, apartment terraces allow interaction with pedestrians. The generous lobby provides an attractive invitation to visitors presenting aspects of the interior of the development to the public. An expanded café space attractively activates the area immediately south of the entry. Services are effectively screened from the street by a feature screen conceived as public art.

5. Public Art

The design guidelines do not specifically require public art work to be integrated into the project. However the significance of the streetscape, particularly in respect to ground level screening elements is considered very important. Hillam Architects will work collaboratively with an artist to integrate art into the screening element. We have an excellent track record in this respect as is well demonstrated in other apartment projects where Stuart Green was the artist. An artist’s input will help create an attractive feature at street level.





1.3 Property Description

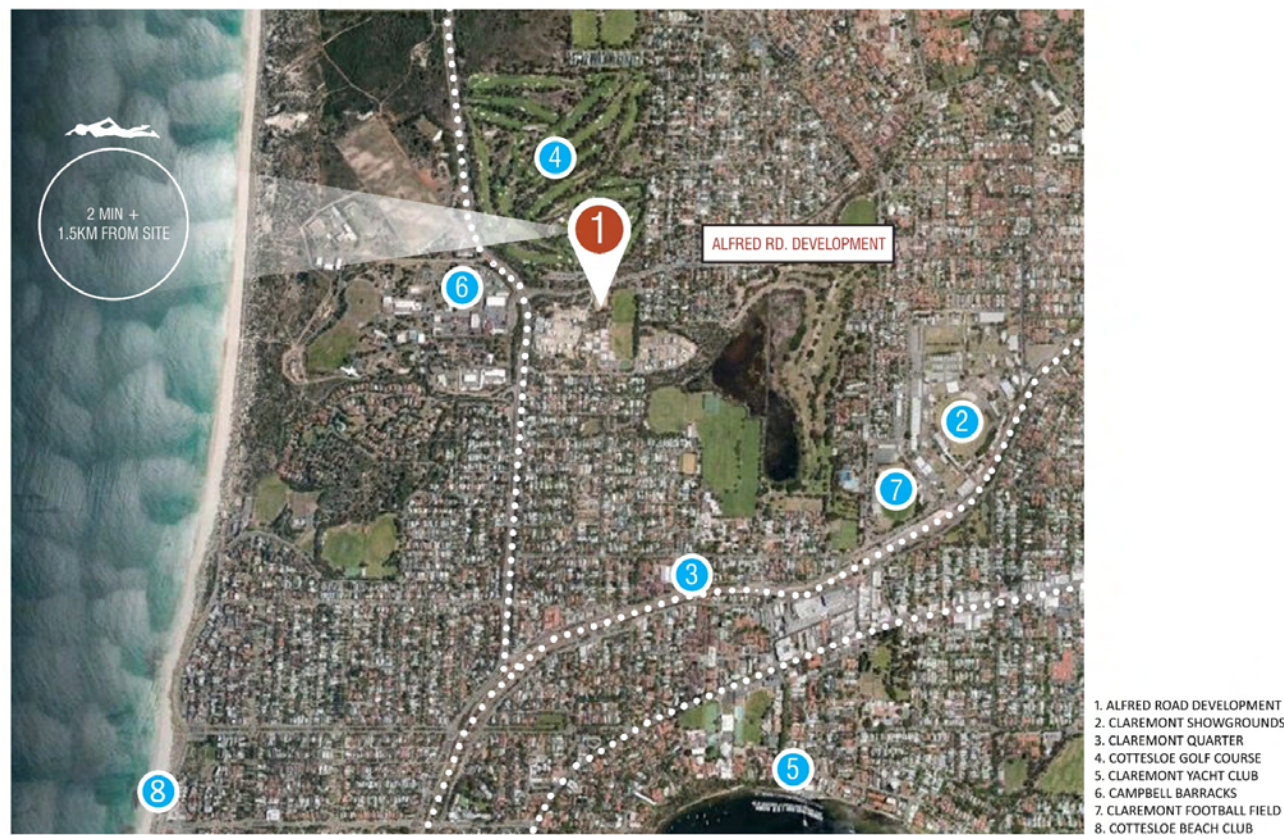
Regional Context

The subject site is located just less than 10 km west of the Perth Central Business District, within the central metropolitan coastal suburb of Swanbourne. The subject site is located within the municipality of the City of Nedlands. The subject site is located just 1600 metres east of the coast and Swanbourne Beach.

In relation to accessibility, the subject site is centrally located to a number of major arterial roads linking to the wider metropolitan region. The subject site lies approximately 250 metres east of the north-south arterial of West Coast Highway. The subject site is also easily connected to Perth CBD via Stirling Highway and Railway Parade. The subject site is located in the vicinity of the Fremantle train line which runs between Perth CBD and Fremantle. It is located approximately 1200 metres from Swanbourne Station, which is eleven stops away from the City Centre.

Local Context

The subject site is located on the corner of Alfred Road and Milyarm Rise. The subject site is situated in an area characterised by a large amount of community and recreational facilities and spaces. Swanbourne Primary School playing fields border to the eastern boundary of the subject site and school grounds extend to the south of the subject site. Public Open Space features both immediately south of the subject site and immediately opposite the subject site on Milyarm Rise. The Cottesloe Golf Course is located the opposite side of Alfred Road. The subject site is located within a new LandCorp / Mirvac residential estate, which was formerly the Swanbourne High School site. A large amount of new residential development has been undertaken or is currently underway within the confines of Alfred Road, West Coast Highway and Narla Road. The area is clearly an upcoming residential locality offering amenity, community, recreation and lifestyle attributes.





2.0 Public Realm

Principles for Development of the Public Realm

Much consideration has been given to the manner in which the project integrates into the public realm. The pedestrian level experience is considered as paramount to the quality of the design. The following important aspects have been considered in the design :

1. Activation

The careful integration of ground floor apartments, café space and openings including the large entry volume provide a diverse frontage to the streets and pedestrian areas. This is further emphasised on other sides of the building not specifically addressing the street. The apartments attractively present to all elevations at both ground level and through the upper levels of development.

2. Safe Places – Crime Prevention Through Environmental Design (CPTED)

Although the ground level frontage is articulated with a series of interesting planes, the space is generally relatively open creating safe public spaces. In addition the setbacks of balconies with varied screening elements above also reinforce the impression of activity and surveillance. Access control systems will provide secure access to apartments and parking areas. Clear signage of pathways, entrances and exits will differentiate public and private spaces.

3. Universal Access

The development will meet all requirements of the Disability Discrimination Act 1992 and the relevant provisions of the Building code of Australia.

3.0 Building Design

The project has been carefully designed to minimize the impact of a large apartment building in an area principally developed with single residential and smaller multi residential developments. Great effort has been made to design a building with significant textural treatments and articulation.

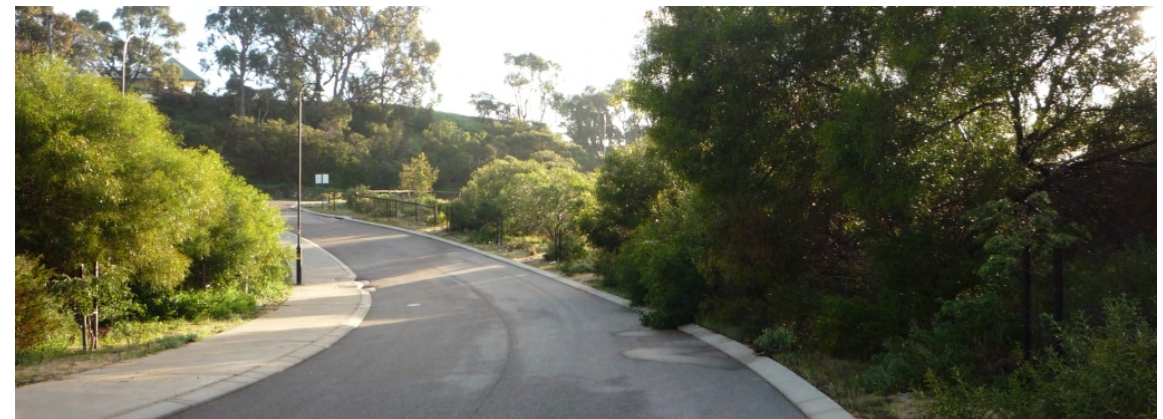
Various architectural treatments have been adopted to break down the scale of the building, both in terms of height and length of the various elevations.

Ventilation is allowed through openings in the lower level structure. This will provide greater amenity to residents enjoying communal areas.

Existing Site Conditions



1. Topography of subject site at Milyarm Rise frontage, rising to the west.



2. Milyarm Rise frontage viewed from the south western corner of the subject site.



3. Subject site looking south east from corner of Alfred Road and Milyarm Rise.



3.1 Architectural Design

1. Milyarm Rise Elevation

The Milyarm Rise elevation is defined with an attractive central double volume entrance softened with timber like screening above, water feature and native landscape. The entrance is accessed by lightly graduated ramps set behind soft curved stone walls and attractive landscape planting.

Differing treatments north and south of the entrance significantly break down the perceived length of the elevation. On the north side the striking white framed elements appropriately punctuate the corner and entrance to the street. The framed elements are contrasted by timber like screens creating movement and interest.

South of the building entry a café space activates the streetscape. Services at ground level are diminished by a textured metal feature screen. Above dramatic bronze screening adds dramatic richness to the elevation. The penthouse level is characterised differently here with timber lining to soffits and expressed roof set forward of parapeted elements.

2. Alfred Road Elevation

A series of vertical framed elements face Alfred Road. These in turn bookend the richer textured screens at the centre. Sliding bronze screens provide effective shading and privacy to the north. The more vertical articulation is reinforced by timber like elements which provide additional texture and interest. A generous landscaping buffer further insulates the apartments from the busier road.

3. East Elevation

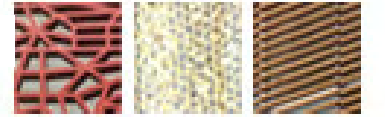
The building has been conceived as a classic base, middle and top with lower levels highly articulated in stone referencing the local landscape and coastal limestone formations. The long elevation, is deliberately split centrally allowing for all residents to access views to the east. In a similar manner to the west elevation the length of the elevation is articulated through a series of framed elements, rich screening, and softer timber elements.

Sky decks are set back to further reduce scale and interest to the top of the building. Extensively planted pergolas provide a green fringe further referencing local bushland and planting.

The combination of vertical and horizontal elements has been designed to minimise the perception of density within the project. Viewed from a distance as will be the case in most instances, it is our view that this composition gives the impression of a lower number of apartments. The fact that distinctly different material bands are also employed further enhances this impression.

4. South Elevation

The south elevation presents to public bushland in a similar manner to the way the north elevation faces Alfred Road. The more vertical framed elements reflect some of the forms of nearby housing. The larger ground floor terrace softens the interface between the bushland and the apartments. All residents will access this space, activating this edge of the development in their enjoyment of the locality.



LINEAR BUILDING & TIMBER FINIS



WHITE RENDER FRAMING



ARCHITECTURAL LANDSCAPE FEATURES



GOLD SCREENING



TIMBER LOURVE



CONTEMPORAY POOL & LOUNGING SPACE

DESIGN REFERENCES



3.2 Materials and Finishes

A varied pallet of materials and finishes articulates the development. A combination of light and dark renders, stone, tile, timber like screening and cladding, perforated metal, translucent sheeting, aluminium shutters and tiles water features provide a cohesive and sophisticated mix. These materials and their subsequent finishes have been selected for their inherent beauty with particular focus being the contrast between the raw and the refined.

METAL SCREENING



TIMBER DECKING



DONNY BROOK STONE



METAL ARTWORK



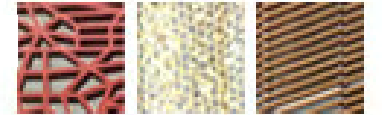
LINED STONE



TIMBER LOOK LOURVES



MATERIAL & FINISHES REFERENCE



3.3 Sustainable Design

Hillam Architects have an excellent track record in providing sustainable apartment buildings in Perth. In particular Verde Apartments in East Perth provides various passive and active sustainable systems delivering positive environmental outcomes.

The Design Guidelines' require the project to achieve a minimum 3 star Nationwide House Energy Rating Scheme (NatHERS) rating. We expect to achieve a minimum of 5 stars NatHers rating.

A summary of some of the additional measures that will be implemented as part of the developments is as follows:

- Various screens are incorporated throughout the building to control sun penetrations into the openings and outdoor living area. (M1)
- The overall building and each dwelling will achieve at least 5 star NatHers rating. (M2)
- Central gas hot water system with solar boosted technology will be installed. (M3)
- Motion sensors will be used to control lightings in car parks and other appropriate areas. (M4)
- A photovoltaic solar energy system has been included to help provide on-site renewable power for the communal components of the building, including circulation corridors, basement lighting, with the light fixtures for these spaces embodying a low-energy efficiency. (M5)
- Provision of energy efficient appliances with minimum 3 star rated to apartment units. The provision excludes oven and rangehood as these appliances do not have energy ratings. (M6)
- Draught seals or weather strips will be installed on openings. (M7)
- 4 stars rated water-wise fixtures and fittings will be installed. 3 stars water-wise shower head will be installed. (M8)
- Agricultural forestry timbers shall be specified throughout. (M9)
- Recycle waste collection are to be provided as per the City of Nedlands' requirement. (M10)
- Suitable provision will be provided to each apartment for waste and recycling. (M11)
- Provision of bicycle storage facilities to encourage tenants to use more environmentally friendly transport alternatives and live an active lifestyle.
- All landscaped areas to be designed for low water requirements in compliance with the design guideline.
- Natural light and ventilation to common corridors to minimise energy consumption.
- Highly insulated structure including if necessary roof, walls and slabs to minimise energy consumption.
- Shading of apartments is supported through the integration of aluminium shutters which are a means to control sun penetration.
- Car park ventilation system controlled by CO2 sensors
- Electrical sub-metering provided for substantial energy uses (eg major plant) to allow for the monitoring and management of significant consumption patterns

Ventilation principles

Bedrooms are provided with operable windows and the interior living spaces open out to the balconies providing all habitable rooms with direct access to fresh air. Mechanical ventilation is incorporated into bathroom spaces that do not have an external facing wall, with additional mechanical ventilation provided to common corridors and basement parking areas.

3.4 Design for Safety

Design element 6.2.1 addresses street surveillance. The deemed-to-comply requirements of the R-Codes for street surveillance are:

- C1.1 The street elevation(s) of the building to address the street with facades generally parallel to the street and with clearly definable entry points visible and accessed from the street.*
- C1.2 The building has habitable room windows or balconies that face the street.*
- C1.3 Basement parking structures between a street frontage and the main front elevation are no more than 1m above natural ground level at any point.*

The proposed development addresses both Milyarn Rise and Alfred Road with facades that generally following the alignment of the street. Entrance points are clearly defined and accessed from Milyarn Rise. Habitable rooms and balconies front to both Milyarn Rise and Alfred Road. Therefore, the proposed development is deemed-to-comply with C1.1 and C1.2 in respect to street surveillance.

Basement parking rises about 2 metres above ground level around the middle of the western elevation (Milyarn Rise), at the northern corner of the western elevation and at the eastern corner of the northern elevation (Alfred Road). The protrusion of the basement parking more than 1 metre above ground level is the result of the varying topography of the subject site, and the elevation of the basement above ground level varies to as low as nil along both street frontages. Thus, the existing topography of the subject site presents a unique context, and this aspect of the street surveillance requirements of the R-Codes should be considered under the design principles for street surveillance.

The applicable design principle for street surveillance is as follows:

- P1.1 Buildings designed to provide for surveillance (actual or perceived) between individual dwellings and the street and between common areas and the street, which minimise opportunities for concealment and entrapment.*

The proposed development has a large number of dwellings overlooking each street frontage that can be expected to minimise concealment opportunities. Additionally, the situation where dwellings on upper levels will look down towards the street will provide surveillance over the select areas where basement walls rise up to 2 metres above ground level. In light of the above, it is considered the proposed development incorporates sufficient street surveillance to comply with the design principle of the R-Codes and should be approved accordingly.



3.5 Fencing

The Design Guidelines specify for Precinct 7 any fencing facing to the street front shall not be higher than 1.8 metres. This measurement shall exclude any retaining wall the fences may be built upon. In regard to retaining walls, new retaining walls shall be constructed in the same material and colours as the existing retaining walls constructed by Mirvac. Any alternate design will have to be considered for approval.

Fences to street front on the proposed development do not exceed 1.2 metres above the ground floor level. Retaining walls are proposed in areas where natural ground levels drop. The proposed retaining walls will be constructed to Mirvac's specifications, or approval for any alterations will be sought. Accordingly, the proposed development can be deemed-to-comply with the fencing requirements of the Design Guidelines and should be approved accordingly.

3.6 Access

The Precinct 7 DAP indicates vehicle access shall be provided from Road 4 (Milyarm Rise) and not Alfred Road. It is further specified by the Precinct 7 DAP that all boundaries other than that fronting to Milyarm Rise shall allow for pedestrian access only.

The proposed development provides vehicle access to basement parking via a 6.1 metre wide driveway at the southern end of the Milyarm Rise street frontage. The primary pedestrian entrance is provided centrally along the Milyarm Rise frontage. Pedestrian access paths also run along the north and south boundaries of the development, as well as centrally through the development to provide access to individual dwellings and communal areas.

Accordingly, the proposed development is deemed to comply with the vehicle and pedestrian access requirements of the Precinct 7 DAP.

3.7 Outdoor Living Areas

Design element 6.3.1 of the R-Codes addresses outdoor living areas. The deemed-to-comply requirement of the R-Codes for outdoor living areas states:

- C1 Each unit is to be provided with at least one balcony or equivalent accessed directly from a habitable room with a minimum area of 10m² and a minimum dimension of 2.4m.*

Each dwelling of the proposed development is provided with a balcony accessible from a habitable room. All dwellings have at least one balcony with a minimum dimension of at least 2.4 metres and a minimum area of at least 10 square metres. As such, the proposed development can be deemed-to-comply with the requirements of the R-Codes for outdoor living areas and warrants approval accordingly.

3.8 Sightlines

Design element 6.2.3 of the R-Codes discusses sight lines at vehicle access points. The deemed-to-comply requirement states:

- C3 Walls, fences and other structures truncated or reduced to no higher than 0.75m within 1.5m of where walls, fences, other structures adjoin vehicle access points where a driveway meets a public street and where two streets intersect, (refer to Figure Series 9).*

3.9 Visual Privacy

Design element 6.4.1 of the R-Codes addresses visual privacy. The deemed-to-comply requirement for visual privacy in areas coded higher than R50 is major openings and unenclosed outdoor active habitable spaces, with a floor level more than 0.5 metres above natural ground level, overlooking any other residential property behind its street setback to be set back from the lot boundary a minimum of:

- 3 metres for major openings to bedrooms and studies;
- 4.5 metres for major openings to habitable rooms other than bedrooms and studies; and
- 6 metres for unenclosed outdoor active habitable spaces.

Alternately, development shall be provided with 75 per cent obscure permanent screening to at least 1.65 metres to restrict overlooking to adjoining properties.

The proposed development does not neighbour to any residential properties. Within the subject site, individual dwellings are distanced adequately to avoid any visual privacy issues. Where balconies adjoin each other, solid walls will provide screening. Accordingly, the proposed development is deemed-to-comply with the requirements of the R-Codes in respect to visual privacy and warrants approval.

3.10 Solar Access

Design element 6.4.2 pertains to solar access for adjoining sites. The deemed-to-comply requirements of the R-Codes in regard to solar access aim to ensure that no development casts an unreasonable shadow over a neighbouring residential property, solar collector, or north facing major openings to habitable rooms. The subject site abuts to POS and primary school playing fields. Therefore, the proposed development does not cause any overshadowing issues and satisfies the deemed-to-comply requirements of the R-Codes.



3.11 Landscaping

The Design Guidelines state careful consideration should go towards achieving landscaping and open space that minimises water use and includes appropriate structures or plants to provide shade in summer and solar access in winter. The Design Guidelines contain a list of recommended water wise plant species for landscaping. The lot owner shall provide reticulation to the adjoin verge as part of its landscaping treatment and will be responsible for maintenance of the verge.

Landscaping provided for the subject site will appropriately consider water usage, shade provision and solar access, using species from the recommended list. It will include landscaping and reticulation of the adjacent verge. The proposal can be considered generally in accordance with the landscaping requirements of the Design Guidelines.

Landscaping Concept

The landscape design for the project draws upon two contrasting worlds: the Perthian bushland setting and the surprise of the exotic.

The project's periphery and its gardens reflect the newly formed estate's native setting. The curve of the site's low entrance wall gives a backdrop and prominence to the estate's weeping peppermint trees at its verge, carpeted under by the purpled flower mass planting of the Dianella, punctuated in the centre by the foyer's water feature. The theme continues with Hardenbergia planters to the north with its purple mist cascading down its walls.

Guided into a courtyard by a series of reflection ponds, the courtyard of the development reveals itself as a mystical surprise. Golden bamboo cascades over stone clad walls, with a lily garden of purple and white flowers. Deep pinks of azaleas are mounded near an open pavilion, opposite to the perfumed southern jasmine garden.

(boundary)
 VERGE PLANTING



Agonis flexuosa

(boundary)
 ENTRANCE STREET PLANTING



Dianella 'Little Rev'

(boundary)
 NORTHERN PLANTER



Hardenbergia violacea 'Meema'

(courtyard)
 BED PLANTINGS



(courtyard)
 FEATURE TREES



Plumeris Obtusa (Frangipani)



Plumeris Obtusa (Frangipani)



Bambusa eutuldoides 'viridi-vittata'



Eucomis comosa 'Oakhurst'



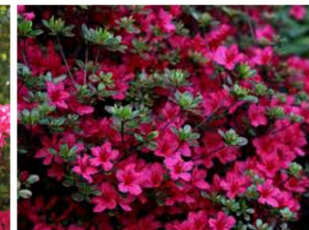
Crinum pendunculata (Swamp Lily)

(courtyard)
 FEATURE PLANTING
 BEHIND STONE WALL

(courtyard)
 PAVILION GARDEN



Azalea var



Azalea var



Acacia cognate 'limelight'



Plumeris Obtusa (Frangipani)



Plumeris Obtusa (Frangipani)



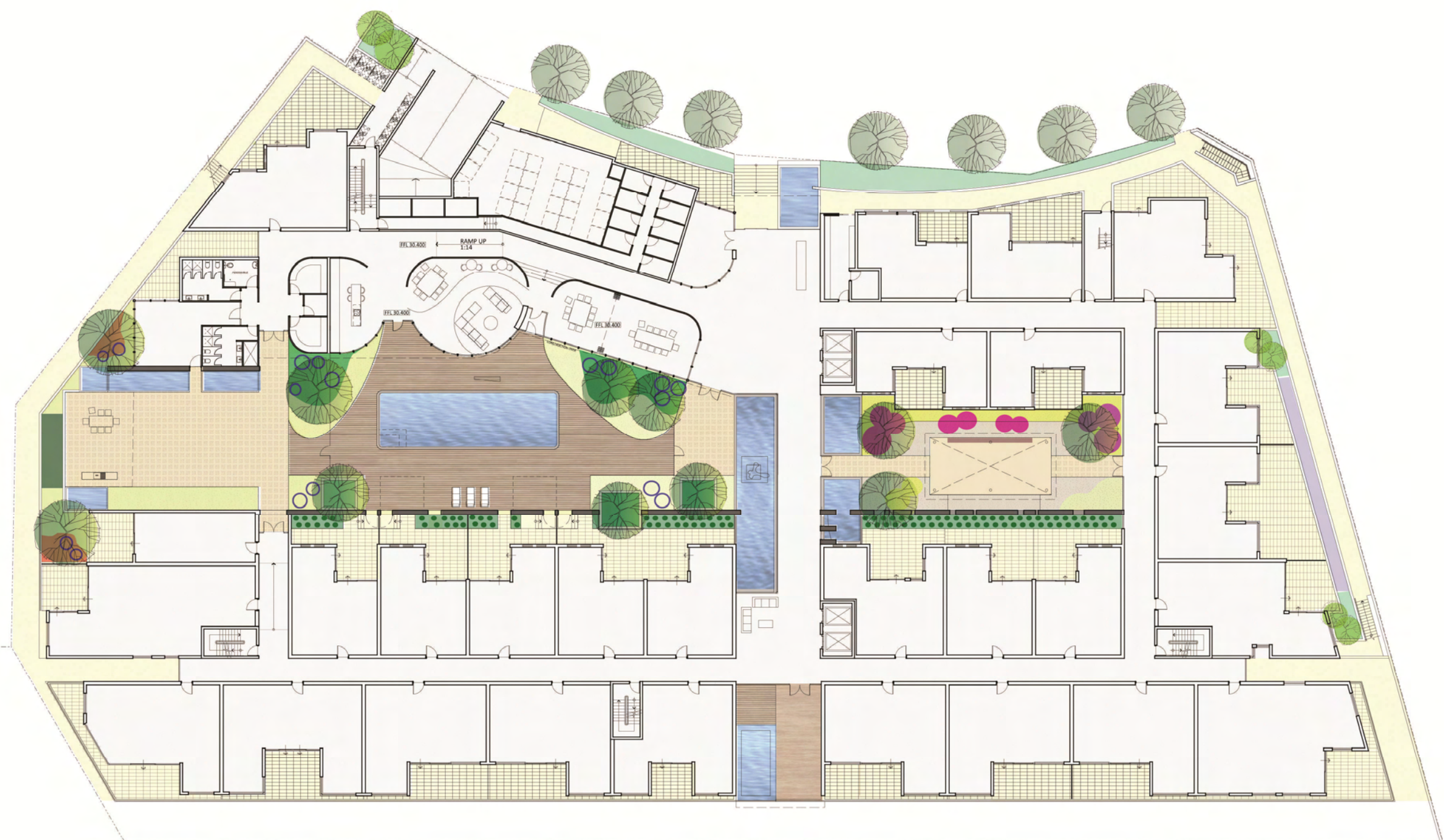
BOUNDARY PLANTING

-  VERGE TREE - *Agonis Flexuosa*
-  ENTRANCE STREET PLANTING - *Acacia rostellifera*
-  ENTRANCE STREET PLANTING - *Dianella 'Little Rev'*
-  NORTHERN PLANTER - *Hardenbergia violacea 'Meema'*

COURTYARD PLANTING

-  FEATURE TREE - *Plumeris Obtusa (Frangipani)*
-  FEATURE PLANTING BEHIND STONE WALL - *Bambusa eutuldoides 'viridi-vittata'*
-  FEATURE BED PLANTINGS - *Eucomis comosa 'Oakhurst'*
-  BED PLANTINGS - *Liriope muscari (Lily Turf)*
-  BED PLANTINGS - *Arthropodium cirratum (Renga Lily)*
-  BED PLANTINGS - *Philodendron 'Xanadu'*
-  BED PLANTINGS - *Trachelospermum jasminoides (Chinese Star Jasmine)*
-  BED PLANTINGS - *Acacia cognate 'limelight'*
-  BED PLANTINGS - *Azalea var*
-  BED INFILL MATERIAL - Crushed Sand

LANDSCAPING GROUND FLOOR PLAN





LANDSCAPING FIFTH FLOOR COMMUNAL SKY GARDEN



FIFTH FLOOR COMMUNAL AREA

SKY GARDEN REFERENCES
CONTEMPORARY GREEN SPACE



ADDITIONAL SEATING AND LOUNGING

ROOFTOP TERRACING

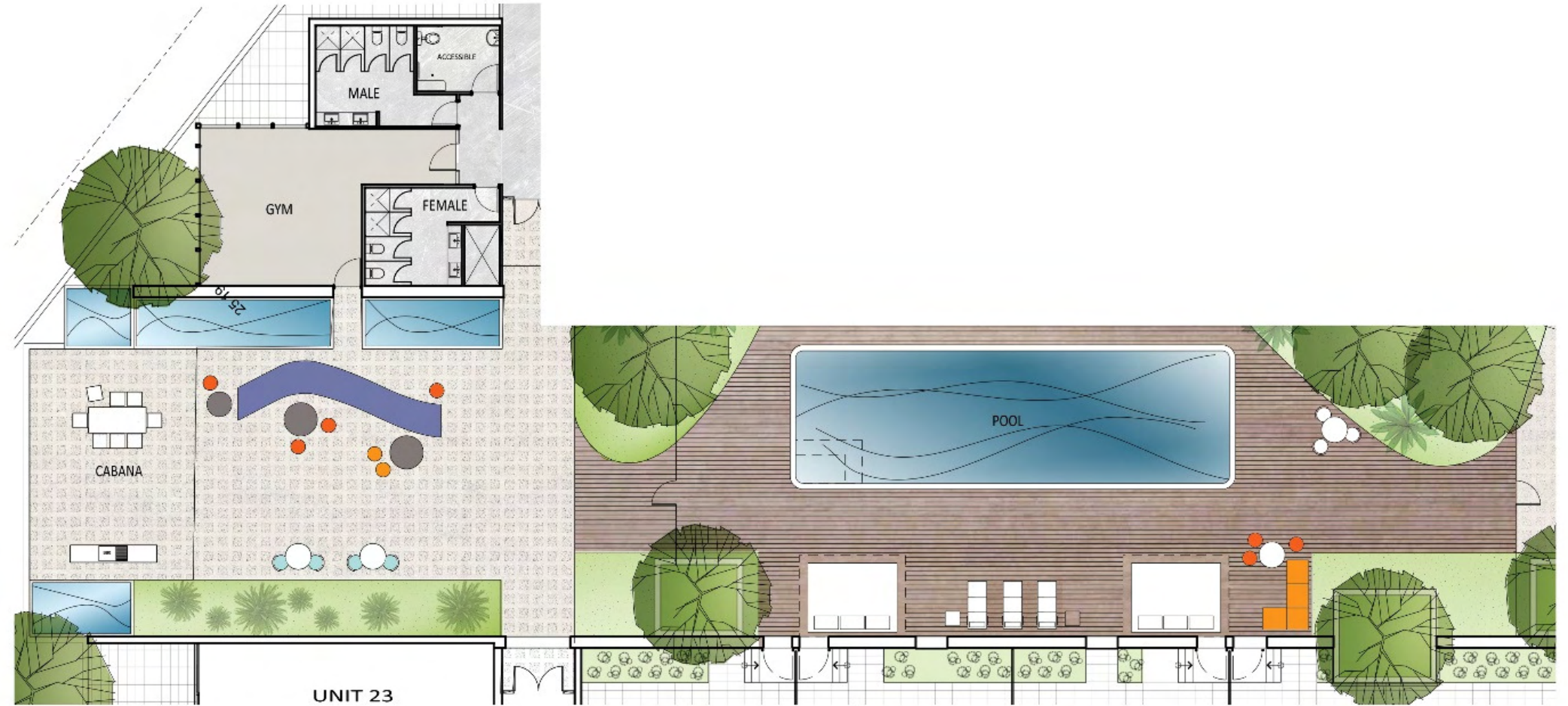


GYMNASIUM AREA WITH VIEWS TO GREEN SPACE



POOL DECK, CABANA AND GYMNASIUM

- Gymnasium, Steam Room and Sauna
- 70sqm Pool & Lounging Area
- Cabana and Barbeque Area
- Japanese Garden & Architectural Landscape Design



GYMNASIUM AREA WITH VIEWS TO GREEN SPACE



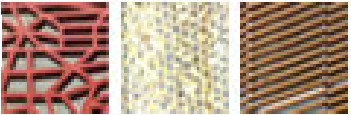
CONTEMPORARY POOL & LOUNGING SPACE



ARCHITECTURAL LANDSCAPE FEATURES

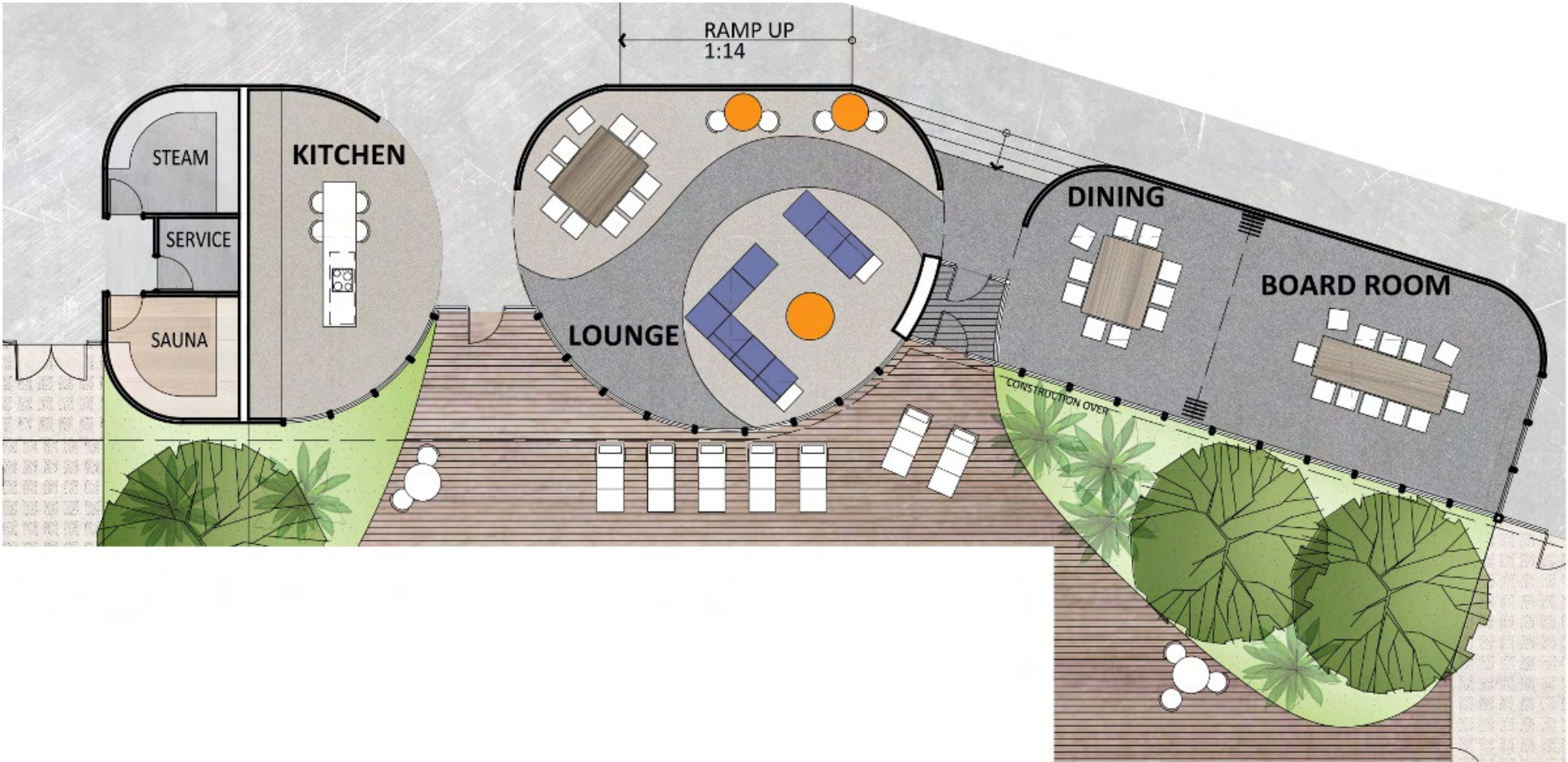


DESIGN REFERENCES



LOUNGE FACILITIES, KITCHEN & BOARDROOM

- Open Kitchen & Breakfast Bench
- Lounge Area & Informal Dining
- 10 PAX Private Dining Room
- 12 PAX Boardroom with Operable Wall



FEATURE CEILING

WARM FINISHES THROUGHOUT LOUNGE AREA

FEATURE LIGHTING OVER DINING AREA



DESIGN REFERENCES



4.0 SITE SPECIFIC GUIDELINES

4.1 Residential Diversity

The project provides an excellent mix of diverse housing. The proposed development consists of a dwelling mix with plot ratio areas as follows:

Two studio dwellings between 44sqm and 48sqm;

Seventy nine bedroom dwellings between 55sqm and 61sqm;

One hundred and six 2 bedroom dwellings between 73sqm and 97sqm; and

Eighteen 3 bedroom dwellings between 108sqm and 160sqm.

Table 1: Simplified summary of housing types

		Basement 2	Basement 1	Ground	1st	2nd	3rd	4th	TOTAL	TOTAL PERCENT
STUDIO (TYPE E)	Standard Studio (40 Strata)	1		1					2	1%
1 x 1 BEDROOM (TYPE A)	Standard 1-bed (50-52 Strata)			4	10	10	10	10	44	21%
	Large 1-bed (55 Strata)	1		7	7	7	7	6	35	17%
2x2 BEDROOM (TYPE C)	Small 2-bed (65-71 Strata)			5	5	5	5	6	26	13%
	Standard 2-bed (72-76 Strata)	7	6	7	10	11	11	5	57	28%
	Large 2-bed (80-85 Strata)	1	1	1	1	2	2	1	9	4%
	Extra Large 2-bed (88 Strata)	1	1	1	3	3	3	2	14	7%
3x2 BEDROOM (TYPE D)	Standard 3-Bed (103-115 Strata)	1	2	2	2	2	2	1	12	6%
PENTHOUSE (TYPE F)	Large 3-Bed (127-150 Strata)							6	6	3%
TOTAL		12	10	28	38	40	40	37	205	100%

4.2 Dwelling Size

Design element 6.4.3 of the R-Codes pertains to dwelling size. The deemed-to-comply requirements with regard to dwelling size state the following:

C3.1 Development that contains more than 12 dwellings are to provide diversity in unit types and sizes as follows:

- *minimum 20 per cent 1 bedroom dwellings, up to a maximum of 50 per cent of the development; and*
- *minimum of 40 per cent 2 bedroom dwellings;*

and

C3.2 The development does not contain any dwelling smaller than 40m² plot ratio area.

The proposed development consists of 205 dwellings in total, 40% of which are single bedroom dwellings and 50% of which are 2 bedroom dwellings. The smallest dwelling in the proposed development is 40 square metres in plot ratio area. As such, the proposed development meets the deemed-to-comply requirements of the R-Codes in respect to dwelling size and should be approved accordingly.

4.3 Proposed Café

A public café is proposed at the residential entrance of the development because there does appear to be a strong desire for this to occur. Currently this is not an approved use. If the café is not approved the space will still address the street as an internal amenity looking outwards.



4.4 Setbacks

The Precinct 7 DAP depicts the setback requirements for the subject site. The setback requirement is 4.0 metres to both Alfred Road and Milyarm Rise street boundaries and 4.0 metres to the eastern lot boundary. The setback requirement for the southern boundary of the site is 2.0 metres. The table below displays the setbacks required by the Precinct 7 DAP against those provided for by the proposal.

WALL	REQUIRED (m)	VARIATION PROPOSED (m)
North (Alfred Rd) – Ground Floor	4.0	0.5 (wall section)
North (Alfred Rd) – 1 st , 2 nd , 3 rd , 4th	4.0	1 (balconies)
North (Alfred Rd) – Basement 1	4.0	1 (wall section) 2.2 (screening wall)
West (Milyarm Rise) – Ground Floor	4.0	0.5 (Public Art Screening and wall sections)
West (Milyarm Rise) - 1 st , 2 nd , 3 rd , 4th	4.0	1 – 1.2 (Balconies) 0.5 (Wall section)
East – Ground Floor	4.0	Nil
East – 1 st , 2 nd , 3 rd , 4th	4.0	0.5-1 (Balconies) 1 (Wall section)
East – Basement 1	4.0	0.5 (Wall section)
East – Basement 2	4.0	0.5 (Wall section) 2.8 (Retaining wall)
South – Ground Floor	2.0	Nil
South – 1 st , 2 nd , 3 rd , 4th	2.0	0.3-1 (Balconies)
South – Basement 1	2.0	0.5 (Wall section)
South – Basement 2	2.0	0.5 (Wall section) 1 (Retaining wall)

The Design Guidelines allow for minor horizontal incursions into the setbacks, provided they are a maximum of 1 metre and cover a maximum 50% of the elevation. Minor incursions are deemed to include items such as balconies with no enclosed space underneath, porches, verandas, blade walls or chimneys.

Many of the incursions into setbacks provided for by the proposed development would be appropriately classified as minor incursions, and can therefore be considered allowable under the requirements of the Design Guidelines. The application, however, seeks further small variations to the setback requirements of the Design

Guidelines to allow for the balconies and irregular building shape proposed to complement the existing lot shape.

The variations sought are considered to be consistent with the objectives of Precinct 7 and the Design Guidelines overall. The proposed development comprises a high level of amenity and character. It promotes passive surveillance with the provision of balconies to all frontages. The variations to setbacks proposed contribute to the substantial articulation achieved on all elevations. And the built form reflects the irregularity of the property's boundaries. As such, the proposed development is submitted to be consistent with the objectives of Precinct 7 and the Design Guidelines and warrants approval accordingly.

SETBACK DIAGRAMS



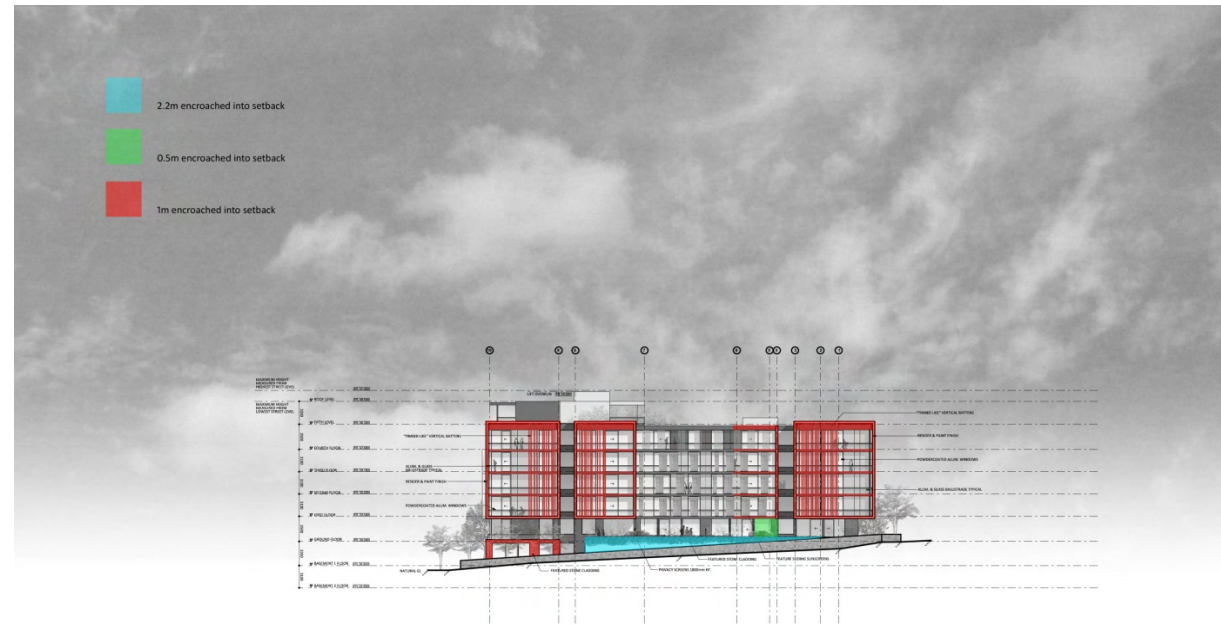
EAST ELEVATION



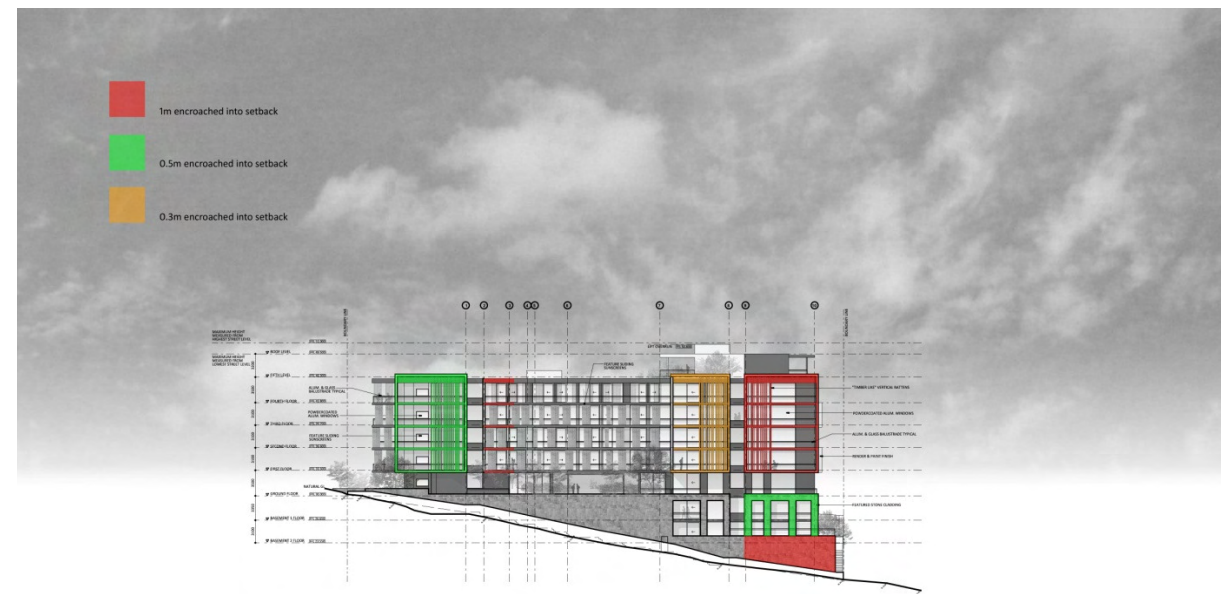
WEST ELEVATION



SETBACK DIAGRAMS (Continued)



NORTH ELEVATION



SOUTH ELEVATION

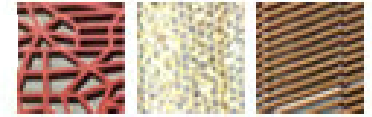
4.5 Open space

The Design Guidelines prescribe a minimum total open space provision of 50% for the subject site. The proposed development provides 3,553 square metres or 54% open space through a combination of ground floor terraces, rooftop terraces, communal open space and other landscaped areas. Accordingly, the proposal provides for adequate provision of open space and warrants support as such.

4.6 Plot ratio

The Design Guidelines state that with respect to the subject site no plot ratio shall apply. The proposed development provides for a plot ratio of 2.43.

It is acknowledged that the R80 density coding would usually provide for a deemed-to-comply plot ratio of 1.0 under the R-Codes. However, the deemed-to-comply plot ratio requirement does not necessarily mean that a higher plot ratio would not be supportable under the design principles of the R-Codes. In this case, the Design Guidelines' statement that no plot ratio shall apply to the subject site makes clear that this aspect of development is intended to be considered qualitatively as a matter of design principles and elements of overall bulk and scale, rather than as a quantitative deemed-to-comply standard. As such, it is submitted that the proposed development's plot ratio is appropriate to its context and affords a building with reasonable scale and bulk for a site that has been identified for high density residential development.



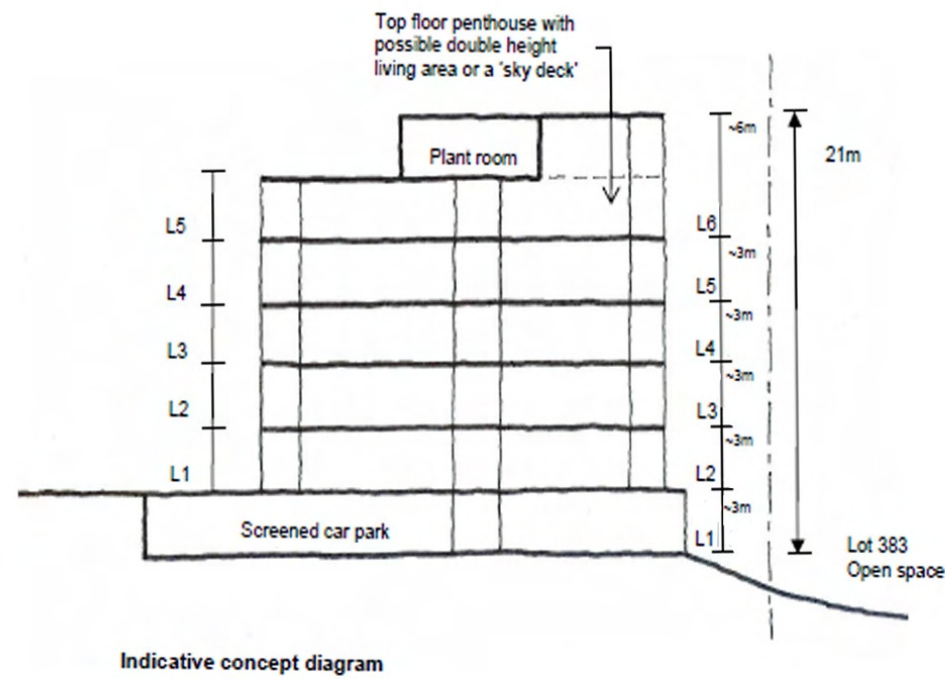
4.7 Maximum Building Height

In regard to maximum building height, the Design Guidelines provide for:

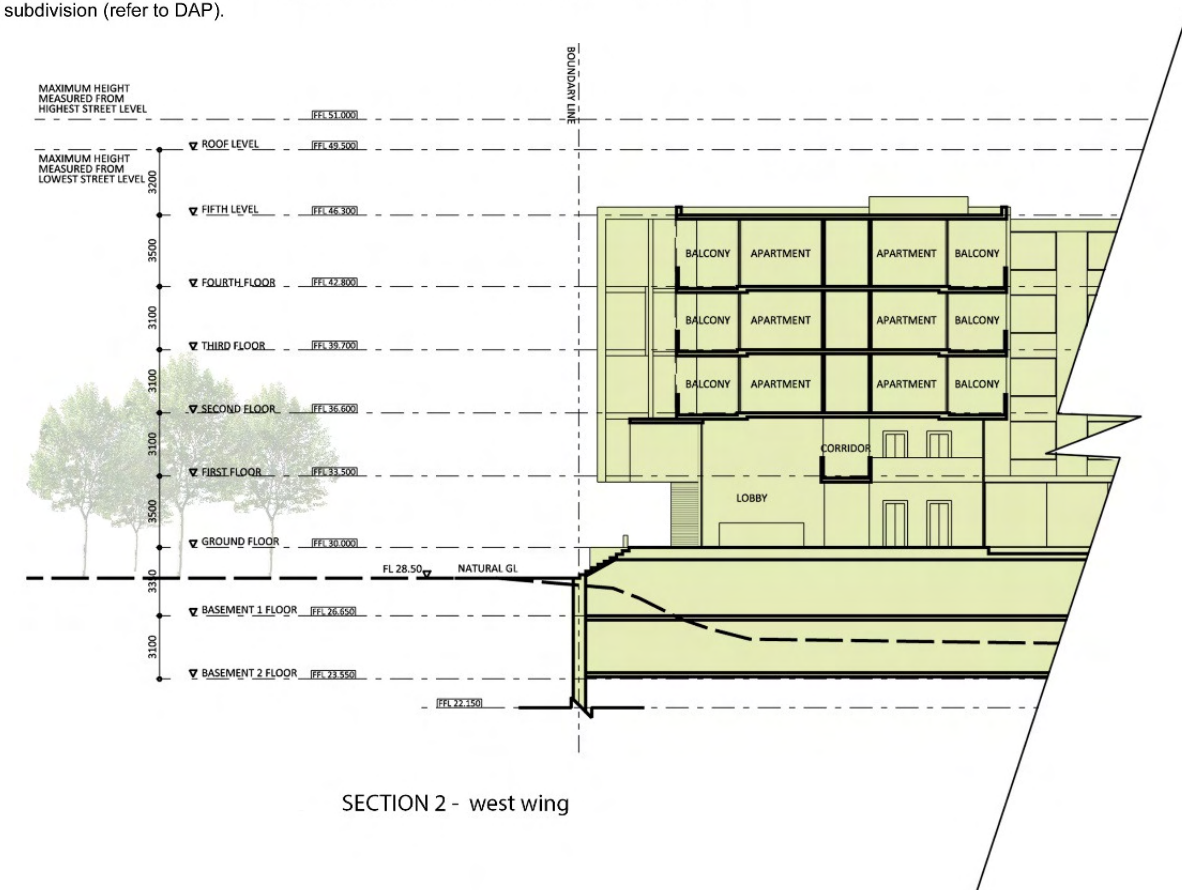
21m and 5 storeys to the street front. The "natural ground level" for measurement of height shall coincide with the finished lot level created at subdivision (refer to DAP).

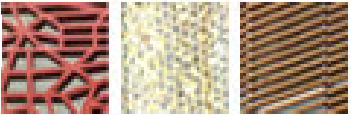
The proposed development consists of 5 storeys to its primary street frontage, Milyarm Rise. Rooftop terraces are proposed above the fourth floor, however, the rooftop terraces are not considered to be an additional storey and the proposed development remains within the overall 21 metre height limit with rooftop terraces included. The proposed development, measured from the primary street front, has a 49.5 metre FFL for the roof over the rooftop terraces, with a small lift overrun at a FFL of 50.8 metres. The natural ground level of the finished lot at the street front ranges between 28.5 metres and 30 metres. Therefore, from the street front, the proposed overall building height measures between 19.5 metres and 21 metres in height. The lift overrun is considered to be acceptable in that it is minor in scale and still within the maximum allowable building height when measured from the highest street level. In light of the above, the proposal is considered to be in accordance with the requirements of the Design Guidelines in respect to maximum building height.

BUILDING HEIGHT DIAGRAM - WEST

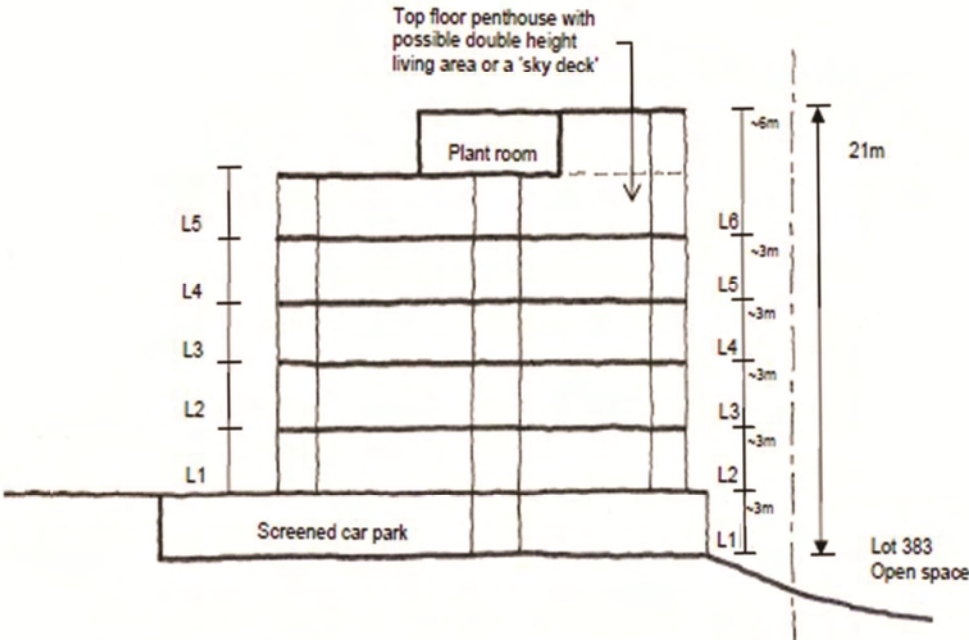


Maximum building height: 21m and 5 storeys from the street front. The "natural ground level" for measurement of height shall coincide with the finished lot level created at subdivision (refer to DAP).





BUILDING HEIGHT DIAGRAM - EAST



Indicative concept diagram





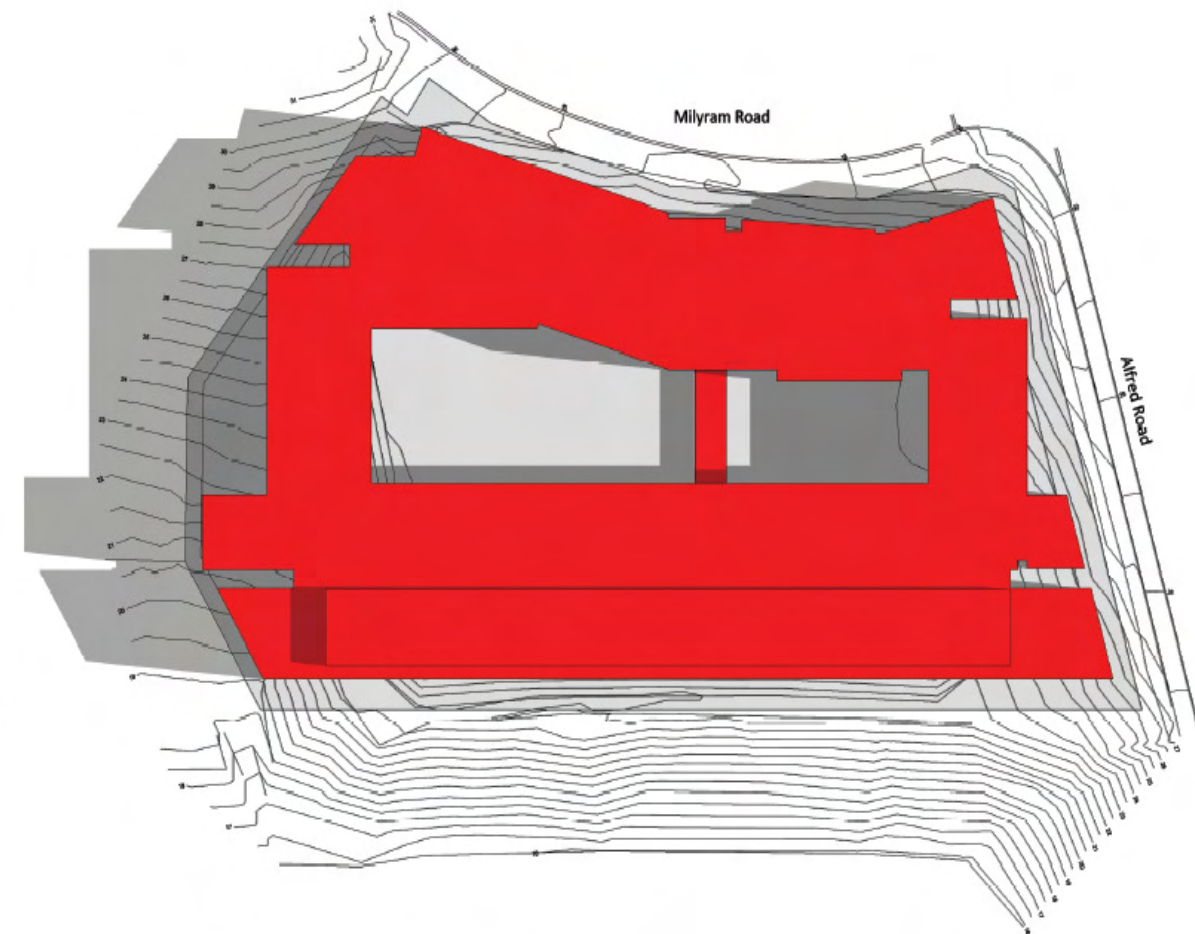
4.8 Overshadowing

The project has little impact in respect to overshadowing. The location of the development is a clear advantage in this respect. Diagrams indicate that at midday on the 21st of June the amount of shadow cast is approximately 75% of the shadow that could be cast by the allowable building envelope. The shadow is cast over bushland and has no direct impact on neighbours.

OVERSHADOWING DIAGRAM



GUIDELINES SCHEME
Area: 2087.34 sqm



PROPOSED SCHEME
Area: 1557.03 sqm

OVERSHADOWING DIAGRAM TAKEN ON THE 21ST JUNE AT 12:00PM



4.9 Carparking

Design element 6.3.3 of the R-Codes pertains to car and bicycle parking requirements. **Table 3** displays the deemed-to-comply requirement of the R-Codes for on-site car parking as compared to the proposed on-site car parking.

Dwelling size	Parking standard	Required car bays	Provided
Small (<75 sqm or 1 bedroom)	1 per dwelling	(95 dwellings) 95 bays	346 Residential bays
Medium (75 – 110 sqm)	1.25 per dwelling	(94 dwellings) 118 bays	
Large (>110m ²)	1.5 per dwelling	(16 dwellings) 24 bays	
Visitors	0.25 per dwelling	(205 dwellings) 52 bays	15 Visitor bays
Total		289 bays	361 bays
Total Surplus			+109 Residential bays
Total Shortfall			-37 Visitor bays

Table 3 demonstrates the proposed development provides for an overall surplus of bays provided as compared to what is required. When broken down, the development proposes a surplus of 110 residential bays and a shortfall of 37 visitor bays. Accordingly, the application seeks a variation to the deemed-to-comply requirement of the R-Codes with regard to visitor parking.

It is considered the visitor parking requirements of Part 6 of the R-Codes significantly overestimate the demand for visitor parking, as they are based on the premise that, at any given time, 1 in 4 dwellings will have a visitor who has driven to the site. It is considered the actual demand for visitor parking, particularly for inner urban multiple dwelling developments, is substantially lower than that envisaged by the R-Codes.

It is submitted the level of car parking provided for by the proposed development is adequate and appropriate to the context. An additional 4 on-street car parking bays are located just south of the subject site on Milyarm Rise. The proposed development is serviced by several adequate quality bicycle routes and Transperth bus routes 381 and 27. The subject site is located approximately 1200 metres from the Swanbourne train station, providing connection to both Perth and Fremantle. Furthermore, with the substantial oversupply of residential bays, it can be expected that some residents may not use both of their car parking bays on a regular basis and will be able to offer visitors the option to park in their second allocated bay when visiting the subject site. Traffic engineers have been engaged to prepare a visitor parking study for mixed-use and multiple dwelling developments across Perth. This study will be submitted to the City to provide further justification in regard to the proposed shortfall to visitor parking requirements.

In respect to bicycles, C3.2 of the R-Codes states the deemed-to-comply requirement for bicycle parking to be one bicycle space for every three dwellings for residents and one bicycle space for every ten dwellings for visitors (provided in accordance with AS2890.3). The proposed development consists of 205 dwellings, and therefore requires 68 bicycle bays for residents and 21 bicycle bays for visitors. The proposed development provides at total of 89 bicycle parking spaces, accommodating the required 68 resident bicycle parking bays and 21 visitor bicycle parking bays. Therefore, the proposed development can be deemed-to-comply with the requirements of the R-Codes for bicycle parking.

4.10 Design of Car Parking Spaces

Design element 6.3.4 of the R-Codes addresses the design of car parking spaces. The deemed-to-comply requirements of the R-Codes are as follows:

- C4.1 *Car parking spaces and manoeuvring areas designed and provided in accordance with AS2890.1 (as amended).*
- C4.2 *Visitor car parking spaces:*
 - *marked and clearly signposted as dedicated for visitor use only, and located close to or visible from the point of entry to the development and outside any security barrier; and*
 - *provide an accessible path of travel for people with disabilities.*
- C4.3 *Car parking areas comprising six or more spaces provided with landscaping between each six consecutive external car parking spaces to include shade trees.*
- C4.4 *All car parking spaces except visitors' car parking spaces fully concealed from the street or public place.*

Car parking provided with the proposal is designed in accordance with Australian standards. Visitor parking is clearly marked and accessible, conveniently to the parking garage entrance on the upper basement level 1. All visitor parking bays will be signposted as such. No car parking bays will be located externally, and therefore landscaping is not necessary. All parking is concealed within the enclosed basement. As such, the proposed development has designed car parking spaces in accordance with the deemed-to-comply requirements of the R-Codes and warrants approval accordingly.

4.11 Vehicle and Pedestrian Access

The Precinct 7 DAP indicates vehicle access shall be provided from Road 4 (Milyarm Rise) and not Alfred Road. It is further specified by the Precinct 7 DAP that all boundaries other than that fronting to Milyarm Rise shall allow for pedestrian access only.

The proposed development provides vehicle access to basement parking via a 6 metre wide driveway at the southern end of the Milyarm Rise street frontage. The primary pedestrian entrance is provided centrally along the Milyarm Rise frontage. Pedestrian access paths also run along the north and south boundaries of the development, as well as centrally through the development to provide access to individual dwellings and communal areas.

Accordingly, the proposed development is deemed to comply with the vehicle and pedestrian access requirements of the Precinct 7 DAP.



4.12 Other requirements

The Design Guidelines provide for a series of other development standards specific to Precinct 7, being the subject site. The various standards are listed and addressed as follows.

- *Habitable loft space and windows in the roof are not permitted. A skylight will be considered subject to appropriate detailing including no openings to be permitted.*

The proposed development does not consist of a habitable loft space. The development rather proposes an unenclosed sky garden space on the roof. It is considered that as the R-Codes definition for a habitable room excludes verandas and unenclosed patios, the sky garden is not to be considered a habitable loft space.

- *The development shall respond to the surrounding context of streetscapes and Public Open Space by orientating elevations (with habitable rooms, openings and balconies) towards these areas.*

A design response that includes large expanses of blank walls and minimal articulation of the elevation and landscape; and any "back of development" appearance (for example, exposed mechanical plant and servicing areas) visible from the public realm will not be accepted.

The proposed development is designed to allow habitable rooms, balconies and openings to front to all four elevations. The design creates active frontages to both Milyarm Rise and Alfred Road, as well as to the school playing fields and Public Open Space reserves to the east and south of the subject site. Servicing areas and plant rooms are not visible from the public realm and all frontages incorporate detailed articulation to create interest no matter what perspective the development is viewed from.

Furthermore, the site offers a unique context in terms of topography, which the proposed development creatively responds to. The building design introduces dwellings at basement level along the east side where ground level drops towards the neighbouring school. The incorporation of dwellings at these levels ensures that active frontages present to the neighbouring school playing fields and avoids large expanses of blank wall that could result from basement parking areas that would actually be above ground along the eastern boundary, and therefore require substantial screening.

- *Residents' car parking shall be screened on site from view of the public realm.*

All residents' car parking is located at basement level and therefore screened from view of the public realm.

- *Maximum of two crossovers from Road 4.*

The proposed development consists of a single crossover to Road 4 (Milyarm Rise).

- *Signage shall be of a high quality graphic design, simple in format and appropriately located and integrated with the building design and landscape to minimise a cluttered appearance. Entry communication and mail delivery boxes shall be discreet and integral to the building design.*

Any signage associated with the development will be of a high quality and located and integrated appropriately within the building design to ensure an attractive and uncluttered appearance. An internal mail room is integrated into the building design.

- *Entrances shall be well lit for safe use for after dark.*

All entrances will be lit appropriately for safe use at night.

- *A screened, central waste storage and collection space shall be included to accommodate bins (in accordance with City of Nedlands' standard waste and recycling system requirements).*

A screened bin storage and collection area is located centrally at ground floor adjacent to the vehicle access point. Waste storage and collection spaces will be provided generally in accordance with the City of Nedlands' standard waste and recycling system requirements. Further details in regard to waste management are contained within the waste management plan prepared for the proposed development and lodged with this development application.

In light of the above, it is clear the proposed development complies with all of the 'other requirements' of the Design Guidelines for Precinct 7 and warrants approval accordingly.



5.0 Servicing and Ancillary

5.1 Services

The Design Guidelines specify:

- *All pipes, wired services, clothes drying areas, hot water storage tanks and such items shall be concealed on the Lot, and shall not be seen from anywhere in the public realm.*
- *Meter boxes shall be located in the least obvious location from public view.*
- *TV antennae, satellite dishes and radio masts shall not be seen from the primary or secondary streets. These items shall be located where they are in the least obtrusive location from the neighbour's property (for example, located at the rear of the roof and below the roof ridge level away from the neighbour's outdoor living area).*
- *Roof and wall mounted air conditioning units are not permitted unless fully concealed.*

The proposed development conceals from public view and appropriately locates all services as required and should be approved accordingly.

5.2 Stormwater

The Design Guidelines require all stormwater to be collected within the lot.

The proposed development will collect all stormwater onsite, and should be approved accordingly.

5.3 Waste and recycling

In regard to waste and recycling, the Design Guidelines require as follows:

- *Waste bin/s shall be stored and concealed from public view on the Lot.*
- *Provision shall be made for the appropriate number of bin pads (in accordance with City of Nedlands' standard waste and recycling system requirements).*

The proposed development provides for a bin store room to store and conceal bins from public view. Bins will be provided in accordance with City of Nedlands' standard waste management and recycling requirements. Further details in regard to waste management are contained within the waste management plan prepared for the proposed development and lodged with this development application.

Refer to the waste management report prepared by Encycle (Appendix 7.5)

5.4 Storage

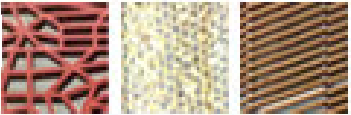
The Design Guidelines state:

- *Commercial vehicle, including caravans, boats, trailers etc. shall not be parked or stored on the Lot unless enclosed within a garage or fully screened from public view.*

The proposed development does not contain areas for storage of commercial vehicles or trailers of any sort and residents would not be expected to park or store such vehicles on the subject site. Any vehicle parked in the residents' allocated basement parking bays will be fully screened from public view. As such, the proposal complies with the Design Guidelines' requirements for storage and should be approved accordingly.

5.5 Encroachment

The design does not propose any encroachment outside the boundaries.



6.0 Swanbourne Design Committee – Comments Received & Resolution

The Swanbourne Design Committee reviewed the development proposal and made several comments to the initial design. This development application proposal has incorporated and addressed the SDC comments as follows:

1. Active Frontage Facing the Public Realm (Western Street)

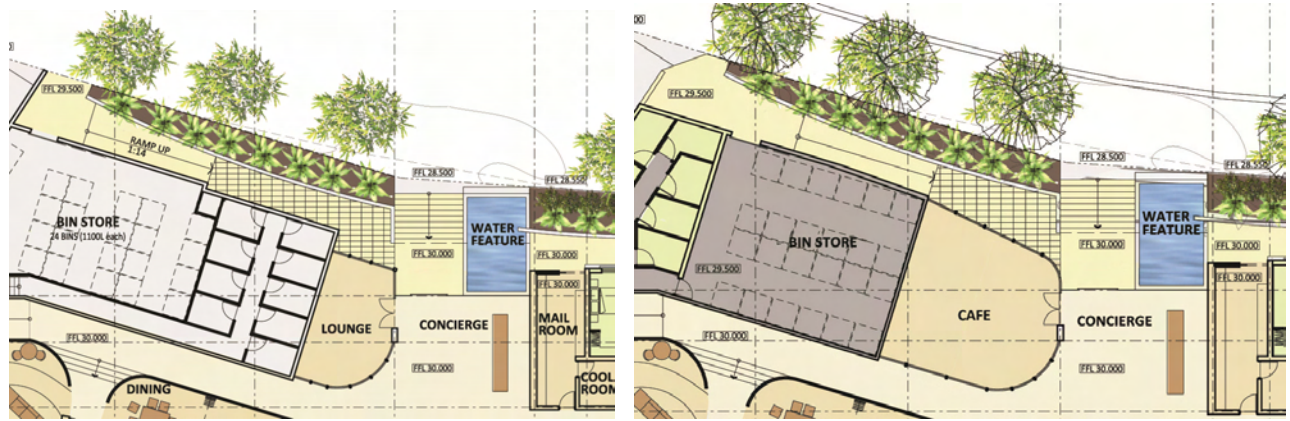
The western elevation at ground level facing the street and south of the main pedestrian entry to the building includes a long length of non habitable blank frontages (stores, centralised bin area and ramp to the lower levels).

It's acknowledged that the ramp location and screened bin store area are required to be adjacent to the street for vehicular access and collection of refuse respectively, however an increase to the active frontage with glazed openings at ground level is sought, particularly through the relocation of the stores at minimum.

It's suggested that a dwelling unit with habitable rooms and openings to be located here facing the street or an appropriate facility for the residents, again with glazed openings to provide visual interest and opportunities for passive surveillance. An alternative design solution would be considered by the SDC to meet this requirement.

The proposed design at ground floor level has been modified to reflect the comments received from the SDC:

- The introduction of a café with full height glazing to Milyarm Rise provides a publicly accessible, active space adjacent to the public realm.
- Full height glazing and a screening canopy emphasise the public entrance point via a concierge adjacent to the café
- Frontage of the screened bin store and store rooms to Milyarm Rise has been minimised



PREVIOUS PROPOSAL

REVISED DESIGN

2. Design of the Fifth Floor (Central Communal Sky Deck)

The central communal skydeck use essentially faces the Perth Cityscape to the west. As per the intent of the section diagram in Precinct 7 b) of the Design Guidelines, a solid screen wall of 2.4m minimum height would meet the intent of the requirement to not permit habitable loft space and windows at roof level with views to the west. An alternative design solution would be considered by the SDC to meet this requirement.

- A communal sky deck / garden is proposed at fifth floor level, with a series of private garden spaces accessible via penthouse apartments at fourth floor level.
- No habitable spaces are proposed at roof level.



PRECEDENT: RESIDENTIAL SKY DECK

3. New Retaining Walls

As per the requirements of the Design Guidelines section on Built Form & Landscape F), any new retaining wall that is visible from the public realm shall be constructed in the same material and colour as the existing walls built by Mirvac (in Limestone; alternative design may be considered for approval).

- No existing retaining walls have been constructed on Lot 60.
- Proposed new retaining walls will match existing walls on adjacent lots within the Mirvac Subdivision

4. Environmental Performance

The SDC would accept a receipt of a report outlining how the design proposal meets the mandatory Environmental Performance requirements noted in the Design Guidelines for Precinct 07 after the DAP meeting at which this application is presented.

- All Environmental Performance Requirements to be addressed with Report to be submitted at Planning Approval Stage



7.0 APPENDIX

7.1 Architectural Drawings

7.2 Perspectives

7.3 Planning Report

7.4 Waste Management Report

7.5 Traffic Management Report

7.6 Swanbourne Design Committee Endorsement Letter and Stamped Drawings.

Proposed Mixed Use Development Lot 60 (2) Alfred Road, Swanbourne

PLANNING SOLUTIONS

URBAN & REGIONAL
PLANNING



July 2013

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Project details

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Prepared by	Planning Solutions	
Consultant Team	Architect	Hillam Architects
	Traffic Engineer	Transcore
	Waste Management	Encycle

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Figure 1:	Regional context
Figure 2:	Local context
Figure 3:	Aerial photograph

Appendices

Appendix 1:	Certificate of Title
Appendix 2:	Development plans

1 Preliminary

1.1 Introduction

Planning Solutions acts on behalf of Alfred Developments Pty Ltd, the proponent of the proposed mixed use development at Lot 60 (102) Alfred Road, Swanbourne (**subject site**). Planning Solutions has prepared the following report in support of an Application for Approval to Commence Development for five storeys of multiple dwellings, plus two levels of basement car parking and dwellings, a rooftop sky garden and ground floor level cafe on the subject site.

This report will discuss various issues pertinent to the proposal, including:

- Site details.
- Proposed development.
- Town planning considerations.

The proposed development will contribute a mix of one, two and three bedroom dwellings that will create a new opportunity for diversification of dwelling types and sizes in the Swanbourne locality. The proposal makes use of a key site allocated for infill development to offer greater residential density in a central metropolitan suburb. The proposed development will also incorporate a cafe located at the ground level entrance to the development that will generate additional activity and vibrancy for the premises. The proposed development responds to the Swanbourne Design Guidelines and Detailed Area Plans prepared specifically for the subject site to facilitate development of high amenity and strong architectural character that is responsive to the unique site context and surroundings. The proposal seeks to satisfy the overall objectives and development standards of the Swanbourne Design Guidelines and Detailed Area Plans, the R-Codes and the City's Town Planning Scheme No. 2.

1.2 Background

The proposed development was presented to the Swanbourne Design Committee (**SDC**), as required by the Swanbourne Design Guidelines, on 27 June 2013. The SDC did not endorse the proposal at this time and recommended some changes to improve the design and achieve the intent of the Swanbourne Design Guidelines. The proposed development was re-presented to the SDC and achieved the SDC's endorsement on 17 July 2013. The SDC confirmed their satisfaction that the design proposed offers a high quality development, that responds positively to its particular site context.

2 Site details

2.1 Land description

Refer to Table 1 below for a description of the land subject to this development application.

Table 1 – Lot details

Lot	Plan/Diagram	Volume	Folio	Area (m ²)
60	57021	2677	497	6552

Refer **Appendix 1** for a copy of the Certificate of Title.

2.2 Location

2.2.1 Regional context

The subject site is located just less than 10 km west of the Perth Central Business District, within the central metropolitan coastal suburb of Swanbourne. The subject site is located within the municipality of the City of Nedlands. The subject site is located just 1600 metres east of the coast and Swanbourne Beach.

In relation to accessibility, the subject site is centrally located to a number of major arterial roads linking to the wider metropolitan region. The subject site lies approximately 250 metres east of the north-south arterial of West Coast Highway. The subject site is also easily connected to Perth CBD via Stirling Highway and Railway Parade. The subject site is located in the vicinity of the Fremantle train line which runs between Perth CBD and Fremantle. It is located approximately 1200 metres from Swanbourne Station, which is eleven stops away from the City Centre.

Refer **Figure 1**, regional context.

2.2.2 Local context

The subject site is located on the corner of Alfred Road and Milyarm Rise. The subject site is situated in an area characterised by a large amount of community and recreational facilities and spaces. Swanbourne Primary School playing fields border to the eastern boundary of the subject site and school grounds extend to the south of the subject site. Public Open Space features both immediately south of the subject site and immediately opposite the subject site on Milyarm Rise. The Cottesloe Golf Course is located the opposite side of Alfred Road. The subject site is located within a new LandCorp / Mirvac residential estate, which was formerly the Swanbourne High School site. A large amount of new residential development has been undertaken or is currently underway within the confines of Alfred Road, West Coast Highway and Narla Road. The area is clearly an upcoming residential locality offering amenity, community, recreation and lifestyle attributes.

Refer **Figure 2**, local context.

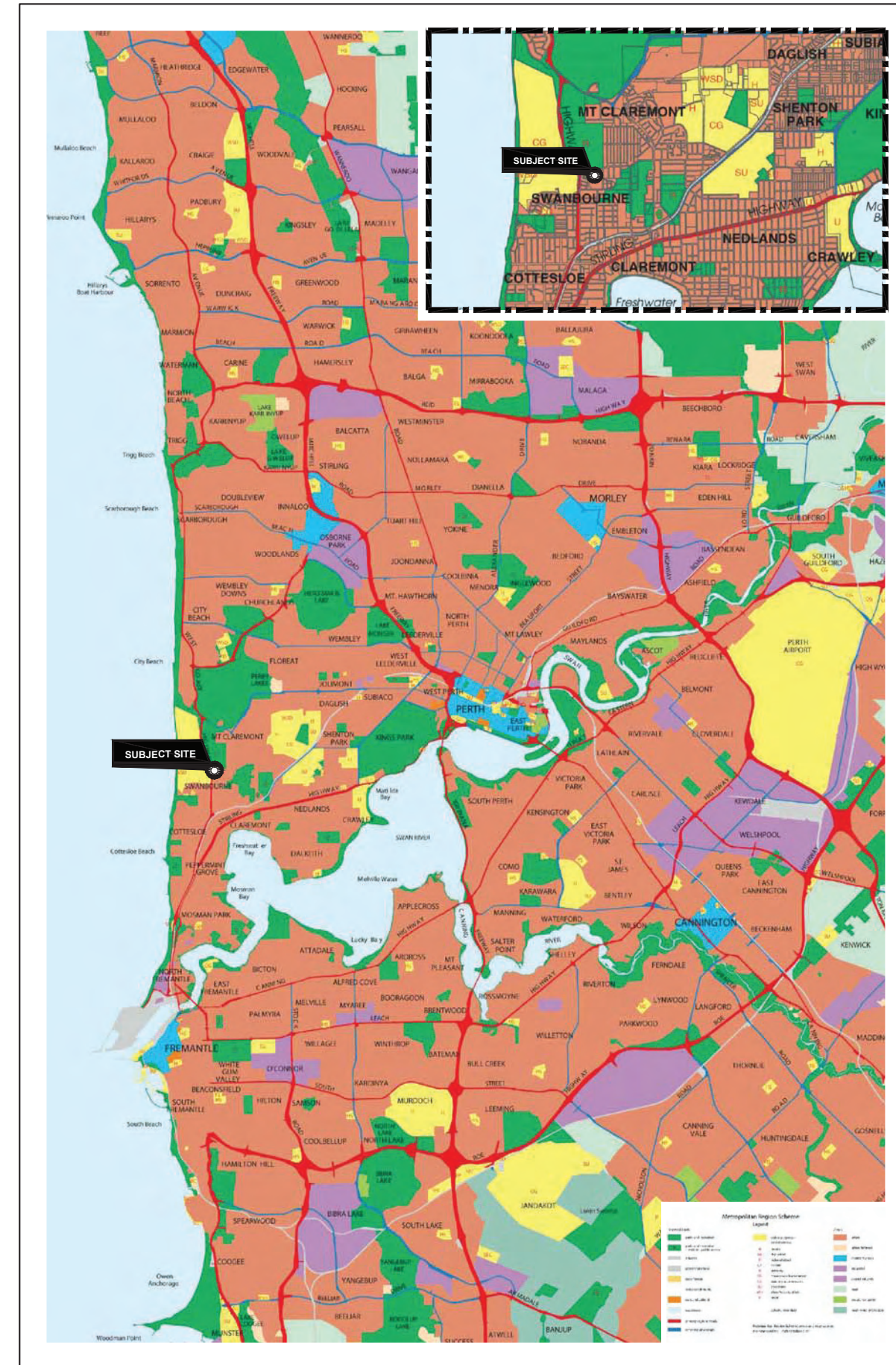


FIGURE 1: REGIONAL CONTEXT - METROPOLITAN REGION SCHEME
LOT 60 (2) ALFRED ROAD
SWANBOURNE
WESTERN AUSTRALIA

SCALE 1:250,000 @ A4
DATE 25 June 2013
FILE 130625 3313 Fig1Reg.dwg
REVISION 1/KG/First Draft/25.06.2013

Scale Bar (m) 0 5000
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BASEPLAN SOURCE: WAPC

2.3 Land use and topography

The subject site is currently vacant land and has been vacant for a considerable period of time since the demolition of the former Swanbourne High School. The subject site contains patches of vegetation and some small trees and shrubs around its periphery, but the majority of the site consists of overgrown grass.

Land directly south and on the western side of Milyarm Rise is reserved for recreation. To the east and further south of the subject site land is reserved for the public purpose of Swanbourne Primary School. To the south west of the subject site further along Milyarm Rise vacant residential land zoned for a density of R60 awaits development. Further to the south west, new residential development consisting of a combination of medium and low densities exists.

The subject site is located on the rise of Alfred Road and has topography that rises quite significantly from east to west. At its highest point, along Milyarm Rise, the land is approximately 30 metres AHD. The south east corner of the property is the lowest lying, at approximately 19 metres AHD. Outside the eastern boundary land drops away steeply. The overall difference in elevation within the subject site is 11 metres. The design of the development proposed works with the topography of the land to create a unique development that responds appropriately to its natural environment.

Photographs 1 to 7 depict the subject site and surrounds.

Refer **Figure 3**, aerial photograph.



Photograph 1 – Subject site looking south east from corner of Alfred Road and Milyarm Rise.

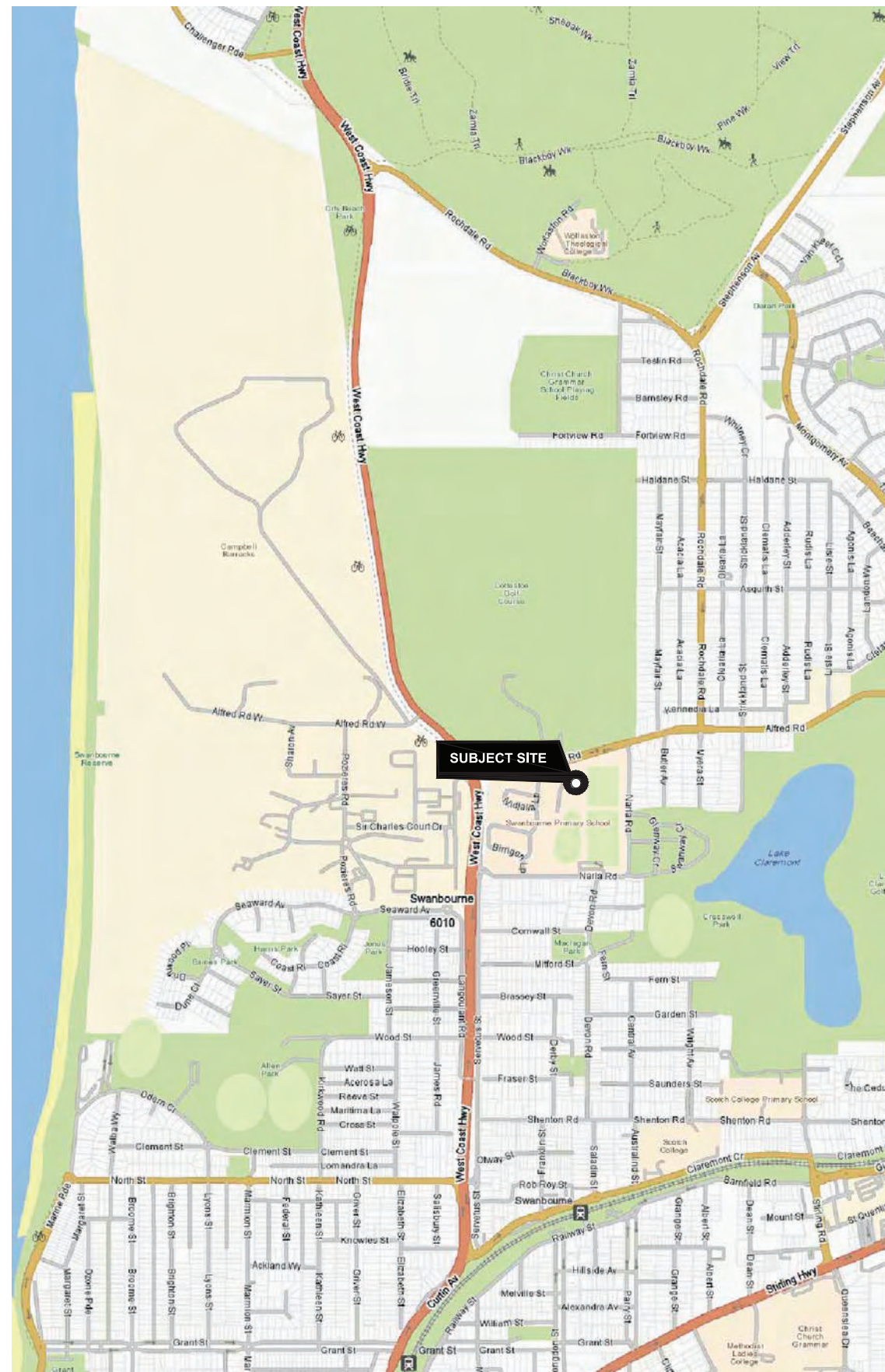


FIGURE 2: LOCAL CONTEXT
LOT 60 (2) ALFRED STREET
SWANBOURNE
WESTERN AUSTRALIA

SCALE NTS @ A4
DATE 18 July 2013
FILE 130718 3313 Fig2Loc.dwg
REVISION 2/GW/Minor mods/18.07.13
1/KG/First Draft/25.06.13

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Photograph 2 – Milyarm Rise frontage viewed from the south western corner of the subject site.



Photograph 3 – Topography of subject site at Milyarm Rise frontage, rising to the west.



Photograph 4 – Looking west along Alfred Road from road reserve north east of subject site.



Photograph 5 – Undeveloped public open space and residential lots in the background immediately opposite subject site on Milyarm Rise.



Photograph 6 – New single dwelling residential development to the south west of subject site.



Photograph 7 – Swanbourne Primary School immediately east of the subject site, looking west towards subject site.

3 Proposed development

This Application for Approval to Commence Development is for a multiple dwelling development comprising a total of 205 dwellings. The proposed development comprises 5 storeys of multiple dwellings entirely above ground level, a roof level with a sky garden and two levels of basement, which consist primarily of parking, but also contain multiple dwellings along the east where the ground level drops significantly.

The proposed development consists of a dwelling mix with plot ratio areas as follows:

- Two studio dwellings between 44sqm and 48sqm;
- Seventy nine 1 bedroom dwellings between 54sqm and 61sqm;
- One hundred and six 2 bedroom dwellings between 73sqm and 97sqm; and
- Eighteen 3 bedroom dwellings between 99sqm and 160sqm.

The layout of the development, with a substantially sized central void is arranged to allow direct sunlight and cross ventilation to units. The units are also designed with large windows to maximise ventilation and natural light access. The total internal floor area (plot ratio area) of the dwellings is 15,775sqm and the overall plot ratio for the development is 2.43.

The proposed development provides for two levels of basement car parking to accommodate both residential and visitor parking. A total of 174 car parking bays are provided for in basement 1, with the provision of 92 single car bays and 41 tandem bays located on this level. A total of 187 car parking bays are provided for in basement 2, with the provision of 91 single car bays and 48 tandem car bays located on this level. In total the proposal provides for 361 car parking bays, 15 of which are designated for visitor use and the remainder of which are residential.

Storage is provided on both levels of basement 1 and basement 2, with 89 stores located in basement level 1 and 104 stores located in basement level 2. Additionally, 12 stores are located at ground level to bring the total number of stores provided to 205.

Access to basement parking is provided by a 6.1 metre wide driveway at the southern end of the Milyarm Rise street frontage. The primary pedestrian entrance is provided centrally along the Milyarm Rise frontage.

Refer **Appendix 2** for a copy of the development plans.



FIGURE 3: AERIAL PHOTOGRAPH
LOT 60 (2) ALFRED STREET
SWANBOURNE
WESTERN AUSTRALIA

SCALE NTS @ A4
DATE 25 June 2013
FILE 130625 3313 Fig3Aer.dwg
REVISION 1/KG/First Draft/25.06.13

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4 Strategic planning framework

4.1 Directions 2031 and Beyond

Directions 2031 and Beyond (**Directions 2031**) is the high-level strategic planning framework for the Perth and Peel region. The Directions 2031 framework proposes five strategic themes for a liveable, prosperous, accessible, sustainable and responsible city. The framework sets out the distribution of housing targets to be met by local governments.

Directions 2031 highlights the subject site to be located in a minor growth area, where future infill development will contribute an additional 3,500 dwellings in the Nedlands locality. Due to the subject site's close proximity to the Secondary Centre of Claremont, the City of Nedlands seeks to promote increased housing diversity within the area while providing more affordable housing options. The subject site is also recognised to be located in close proximity to the UWA – QEII Specialised Centre and the Stirling Highway urban corridor. It is accordingly considered the subject site is a valuable location for high residential development. It is submitted the proposed development would provide a diversity of residential options on a significantly underutilised land resource, which is located conveniently to a variety of services and amenities.

The proposed development is consistent with the strategic objectives of Directions 2031 in that it promotes higher density infill residential development in the Swanbourne locality. The proposed development will aid in the diversification of housing options and supply increased residential densities, as called for under the strategic framework of Directions 2031.

4.2 Central Metropolitan Perth Sub-Regional Strategy

The (draft) Central Metropolitan Perth Sub-Regional Strategy (**Sub-Regional Strategy**) provides more in depth strategic planning for the growth of the Central Metropolitan Perth Region in order to deliver the outcomes sought by Directions 2031.

Under the Sub-Regional Strategy the City of Nedlands is required to increase its existing housing stock of 8,260 dwellings to achieve a target of an additional 3,500 dwellings. Whilst it is acknowledged that careful planning is required to preserve streetscapes and neighbourhood character, new housing is required in a compact and sustainable urban form, which promotes housing choice and diversity in response to changing community needs. It is recognised to be important to optimise the use of large 'strategic' sites in order to achieve infill targets whilst maintaining traditional residential character.

The Sub-Regional Strategy identifies a crucial role for private sector developers to invest in higher density housing projects and for Local Government to encourage innovative infill and be advocates for the housing needs of future generations. Accordingly, the proposed high density multiple dwelling development is clearly in line with the strategic vision of the Sub-Regional Strategy.

5 Statutory planning framework

5.1 Metropolitan Region Scheme

The subject site is zoned Urban under the Metropolitan Region Scheme (**MRS**).

The proposed development is consistent with the provisions of the MRS and may be approved accordingly.

5.2 City of Nedlands Town Planning Scheme No. 2

5.2.1 Zoning

The subject site is zoned Residential with an applicable density of R80 under City of Nedlands' Town Planning Scheme No. 2 (**TPS2**). The subject site abuts to land reserved under TPS2 for the purposes of Primary School and Recreation.

5.2.2 Use

Under the provisions of TPS2, the use Dwelling House – Multiple is an 'X' use in the Residential zone. However, Clause 5.3.11 of TPS2 states:

5.3.11 *Notwithstanding the provisions of the Residential Design Codes or any other provision specified elsewhere in the scheme, the following development standards shall apply to the specified lots within the subdivision formerly known as Swanbourne High School:*

- a) ...;
- b) *In respect of proposed Lot 60 a height restriction of 21m shall apply;*
- c) *In respect of proposed Lots 60 and 61 multiple dwellings shall be permitted;...*

As such, TPS2 makes special consideration to provide for and encourage the development of multiple dwellings on the subject site. The proposed use is therefore consistent with the provisions of TPS2 and warrants approval accordingly.

The cafe use can be defined under the provisions of TPS2 as a restaurant. Clause 1.8 of TPS2 states the interpretation of a restaurant as follows:

Restaurant - *means a building wherein food is prepared solely for sale and consumption within the building or portion thereof and (without limiting the generality of the foregoing) the expression includes a licensed restaurant, or cafe. The expression also includes a restaurant at which food for consumption outside the building, or portion thereof, is sold where the Council is of the opinion that the sale of food for consumption outside the building is not the principal part of the business. The expression shall also include an outdoor establishment and in that case for the purpose of this definition, the outdoor eating area shall be treated as being within the building of the restaurant;*

Under the provisions of TPS2, the use Restaurant is an 'X' use in the Residential zone. Despite the Restaurant use being a not permitted use in the Residential zone, it is submitted that the proposed Restaurant use, in the context of being a small onsite cafe that forms a very small proportion of the

overall use of the site is appropriate and supportable. The cafe is proposed to be located at the ground level entrance to the proposed development and will accordingly generate activity and create unique character to this important area of primary Milyarm Rise frontage. The active cafe use assists to mitigate the visual impact of the vehicular entry ramp, bin and storage areas screened from public view at the Milyarm Rise frontage, and will provide a service to residents and visitors of the premises.

In light of the above, it is submitted the proposed Restaurant Use is incidental and ancillary to the predominantly residential development, that will work complementarily with the proposed Multiple Dwelling use to increase the amenity of the proposed development overall. It is requested the Restaurant use be supported on this basis.

5.3 Swanbourne Design Guidelines B and Detailed Area Plans

The City of Nedlands (**City**) has adopted the Swanbourne Design Guidelines (**Design Guidelines**) and Detailed Area Plans (**DAPs**) as a Local Planning Policy under Town Planning Scheme No. 2. The Statutory Requirements section of the Design Guidelines state:

In determining any application for planning approval, the Council will utilise these Guidelines and the DAPs as the primary assessment criteria to be exercised in conjunction with the Scheme and Policies and R-Codes. Where these design guidelines are silent, the Scheme and Residential Design Codes come into effect.

Accordingly, this development application report considers and addresses the development standards of the Design Guidelines firstly and refers to the R-Codes and the City's other Local Planning Policies for any development requirements not addressed by the Design Guidelines.

In regard to variations to development standards, the Design Guidelines state:

The Council may approve an application involving departures from the specific criteria of the Guidelines, if in its opinion, the application satisfies the objectives of the Guidelines. The Council may refuse Planning Approval for a development not considered to be in keeping with the objectives of the guidelines.

Thus, in instances where a variation to the development standard prescribed by the Design Guidelines arises, the objectives of the Design Guidelines are referred to. The objectives promoted by the Design Guidelines are as follows:

- *development that has regard for the amenity of adjacent Lots and surrounding public areas.*
- *a strong sense of architectural character that is distinctly contemporary and designed with integrity, and reflects different dwelling types and the characteristics of modern lifestyles.*
- *residential development that is responsive to the site and context, incorporates passive solar design, achieves thermal comfort and ventilation, is energy efficient, and promotes sustainable water management practices.*
- *opportunities for passive surveillance of the public realm, yet respecting individual privacy.*

The proposed development demonstrates high regard for the amenity of its surrounding environment by providing activated and engaging frontages to all elevations. The design increases the amenity of

surrounding lots and public areas by offering a visually appealing built form that will bring activity to a currently unutilised piece of land. It is expected development of the subject site would instigate development of the Public Open Space reserve located opposite the subject site on Milyarm Rise, therefore further increasing the amenity of the surrounding area.

The proposed development exhibits a strong sense of contemporary architectural character. The proposal offers a wide variety of dwelling sizes and styles, ranging from 44 square metre plot ratio area studios to 3 bedroom 160 square metre apartments. It is submitted that the proposed development's provision of multiple dwellings responds to the evolving modern lifestyle and resultant increased demand for this less traditional style of dwellings.

The proposal responds to the natural topographical characteristics of its site. The design includes dwellings in the two basement levels where the ground level drops away along the property's eastern boundary. This ensures an active and articulated frontage is maintained on the entire eastern elevation, rather than having large expanses of wall screening car parking, or filling the site substantially.

The proposed development is expected to exceed the requirements of the Design Guidelines by achieving a minimum five star Nationwide House Energy Rating Scheme (**NatHERS**) rating. Sustainable design will include a range of both passive and active sustainable systems to deliver positive environmental outcomes. All habitable rooms are provided with direct access to natural light and fresh air with operable windows and balconies. Natural light and ventilation will also be provided to common corridors will minimise energy consumption. Screening will be utilised to control sun penetration into openings and balconies. The development will also incorporate water-wise fixtures and landscaping.

The layout of the proposed development ensures passive surveillance over the public realm on all frontages. Dwellings with balconies and openings overlook both primary and secondary street frontages, as well as adjacent Public Open Space and school playing fields. Without any adjacent residential properties, the proposed development avoids privacy issues associated with overlooking neighbouring properties. Individual privacy between dwellings within the subject site is ensured via appropriate setbacks and solid walls screening between immediately neighbouring balconies.

In light of the above, it is clear that the proposed development successfully achieves the objectives of the Design Guidelines and should be supported accordingly.

5.3.1 Site planning requirements for Precinct 7

The subject site comprises a single lot designated as Precinct 7 on the Swanbourne Precinct Plan and Detailed Area Plans.

The objectives for development of Precinct 7, as stated by the Design Guidelines, are:

- I. a built form that includes articulation of all elevations. Architectural features such as contrasting materials and colours, awnings, canopies, balconies and bay windows shall provide visual interest.*
- II. a response to the surrounding context of Public Open Spaces and streetscapes (refer to the section on Built Form and Landscape).*
- III. passive surveillance opportunities of the surrounding Public Open Spaces.*

IV. *a built form that is faceted to follow the curve of the street front boundary and other common boundaries with some variations in building setback.*

The proposed development contains articulation to all elevations, using contrasting colours, varying materials, balconies, openings and other detailed design features. It addresses all Public Open Space and street frontages with interesting and activated elevations to contribute both amenity and passive surveillance. The proposed built form responds to Milyarm Rise with a customised design to follow the street's curvature. The design mimics the lot boundaries on all sides to create a unique and visually appealing development. As such, it is submitted the proposed development is consistent with the objectives for Precinct 7.

The Design Guidelines and DAP contain several development standards applicable specifically to Precinct 7. These are addressed as follows in this section.

Setbacks

The Precinct 7 DAP depicts the setback requirements for the subject site. The setback requirement is 4.0 metres to both Alfred Road and Milyarm Rise street boundaries and 4.0 metres to the eastern lot boundary. The setback requirement for the southern boundary of the site is 2.0 metres. **Table 2** below displays the setbacks required by the Precinct 7 DAP against those provided for by the proposal.

Table 2 – Street, side and rear boundary setbacks

Wall	Required (m)	Variation Proposed (m)
North (Alfred Road) – ground floor	4.0	0.5 (wall section)
North (Alfred Road) – first, second, third and fourth floors	4.0	1 (balconies)
North (Alfred Road) – basement 1	4.0	1 (wall section) 2.2 (screening wall)
West (Milyarm Rise) – ground floor	4.0	0.5 (public art screening and wall sections)
West (Milyarm Rise) – first, second, third and fourth floors	4.0	1 – 1.2 (balconies) 0.5 (wall section)
East – ground floor	4.0	Nil
East – first, second, third and fourth floors	4.0	0.5 – 1 (balconies) 1 (wall sections)
East – basement 1	4.0	0.5 (wall section)
East – basement 2	4.0	0.5 (wall section) 2.8 (retaining wall)
South – ground floor	2.0	Nil
South – first, second, third and fourth floors	2.0	0.3 – 1 (balconies)
South – basement 1	2.0	0.5 (wall section)
South – basement 2	2.0	0.5 (wall section) 1 (retaining wall)

The Design Guidelines allow for minor horizontal incursions into the setbacks, provided they are a maximum of 1 metre and cover a maximum 50% of the elevation. Minor incursions are deemed to include items such as balconies with no enclosed space underneath, porches, verandas, blade walls or chimneys.

The majority of the incursions into setbacks provided for by the proposed development would be appropriately classified as minor incursions of balconies or terrace walls that are no more than 1 metre, and can therefore be considered allowable under the requirements of the Design Guidelines. The application, however, seeks further small variations to the setback requirements of the Design Guidelines to allow for screening and retaining walls as required to work with the existing lot shape and topography.

The variations sought are considered to be consistent with the objectives of Precinct 7 and the Design Guidelines overall. The proposed development comprises a high level of amenity and character. It promotes passive surveillance with the provision of balconies to all frontages. The variations to setbacks proposed contribute to the substantial articulation achieved on all elevations, and the built form reflects the irregularity of the property's boundaries. As such, the proposed development is submitted to be consistent with the objectives of Precinct 7 and the Design Guidelines and warrants approval accordingly.

Open space

The Design Guidelines prescribe a minimum total open space provision of 50% for the subject site. The proposed development provides 3,553 square metres or 54% open space through a combination of ground floor terraces, rooftop terraces, communal open space and other landscaped areas. Accordingly, the proposal provides for adequate provision of open space and warrants support as such.

Plot ratio

The Design Guidelines state that with respect to the subject site no plot ratio shall apply. The proposed development provides for a plot ratio of 2.43.

It is acknowledged that the R80 density coding would usually provide for a deemed-to-comply plot ratio of 1.0 under the R-Codes. However, the deemed-to-comply plot ratio requirement does not necessarily mean that a higher plot ratio would not be supportable under the design principles of the R-Codes. In this case, the Design Guidelines' statement that no plot ratio shall apply to the subject site makes clear that this aspect of development is intended to be considered qualitatively as a matter of design principles and elements of overall bulk and scale, rather than as a quantitative deemed-to-comply standard. As such, it is submitted that the proposed development's plot ratio is appropriate to its context and affords a building with reasonable scale and bulk for a site that has been identified for high density residential development.

Maximum building height

In regard to maximum building height, the Design Guidelines provide for:

21m and 5 storeys to the street front. The "natural ground level" for measurement of height shall coincide with the finished lot level created at subdivision (refer to DAP).

The proposed development consists of 5 storeys to its primary street frontage, Milyarm Rise. Rooftop terraces are proposed above the fourth floor, however, the rooftop terraces are not considered to be an additional storey and are setback from Milyarm Rise so that they are not visible from the street frontage. The proposed development remains within the overall 21 metre height limit even with rooftop terraces included. The proposed development, measured from the primary street front, has a 49.5 metre FFL

for the roof over the rooftop terraces, with a small lift overrun at a FFL of 50.8 metres. The natural ground level of the finished lot at the street front ranges between 28.5 metres and 30 metres. Therefore, from the street front, the proposed overall building height measures between 19.5 metres and 21 metres in height. The lift overrun is considered to be acceptable in that it is minor in scale and still within the maximum allowable building height when measured from the highest street level. In light of the above, the proposal is considered to be in accordance with the requirements of the Design Guidelines in respect to maximum building height.

In respect to the height of the proposed development at the eastern elevation, the overall building height remains the same, but natural ground level drops away significantly. The perceived height of the building when viewed from the eastern elevation is appropriate, in the context of the other surrounding residences that are setback approximately 140 metres from the subject site, and screened by significant and mature vegetation both immediately east of the eastern property boundary and along Narla Rise.

It is further considered that the height of the proposed development satisfies the objectives of development for Precinct 7, with the objectives for Precinct 7 being:

- I. *a built form that includes articulation of all elevations. Architectural features such as contrasting materials and colours, awnings, canopies, balconies and bay windows shall provide visual interest.*
- II. *a response to the surrounding context of Public Open Spaces and streetscapes (refer to the section on Built Form and Landscape).*
- III. *passive surveillance opportunities of the surrounding Public Open Spaces.*
- IV. *a built form that is faceted to follow the curve of the street front boundary and other common boundaries with some variations in building setback.*

The height of the proposed development responds to the site features and surrounding context, providing a development that makes efficient use of a significantly sloping site to provide an architecturally pleasing design that provides for passive surveillance over all surrounding Public Open Spaces. It is submitted the proposal is accordingly consistent with the objectives for Precinct 7 and warrants approval accordingly.

Vehicle and pedestrian access

The Precinct 7 DAP indicates vehicle access shall be provided from Road 4 (Milyarm Rise) and not Alfred Road. It is further specified by the Precinct 7 DAP that all boundaries other than that fronting to Milyarm Rise shall allow for pedestrian access only.

The proposed development provides vehicle access to basement parking via a 6 metre wide driveway at the southern end of the Milyarm Rise street frontage. The primary pedestrian entrance is provided centrally along the Milyarm Rise frontage. Pedestrian access paths also run along the north and south boundaries of the development, as well as centrally through the development to provide access to individual dwellings and communal areas.

Accordingly, the proposed development is deemed to comply with the vehicle and pedestrian access requirements of the Precinct 7 DAP.

Other requirements

The Design Guidelines provide for a series of other development standards specific to Precinct 7, being the subject site. The various standards are listed and addressed as follows.

- *Habitable loft space and windows in the roof are not permitted. A skylight will be considered subject to appropriate detailing including no openings to be permitted.*

The proposed development does not consist of a habitable loft space. The development rather proposes an unenclosed sky garden space on the roof. It is considered that as the R-Codes definition for a habitable room excludes verandas and unenclosed patios, the sky garden is not to be considered a habitable loft space.

- *The development shall respond to the surrounding context of streetscapes and Public Open Space by orientating elevations (with habitable rooms, openings and balconies) towards these areas. A design response that includes large expanses of blank walls and minimal articulation of the elevation and landscape; and any "back of development" appearance (for example, exposed mechanical plant and servicing areas) visible from the public realm will not be accepted.*

The proposed development is designed to allow habitable rooms, balconies and openings to front to all four elevations. The design creates active frontages to both Milyarm Rise and Alfred Road, as well as to the school playing fields and Public Open Space reserves to the east and south of the subject site. Servicing areas and plant rooms are not visible from the public realm and all frontages incorporate detailed articulation to create interest no matter what perspective the development is viewed from.

Furthermore, the site offers a unique context in terms of topography, which the proposed development creatively responds to. The building design introduces dwellings at basement level along the east side where ground level drops towards the neighbouring school. The incorporation of dwellings at these levels ensures that active frontages present to the neighbouring school playing fields and avoids large expanses of blank wall that could result from basement parking areas that would actually be above ground along the eastern boundary, and therefore require substantial screening.

- *Residents' car parking shall be screened on site from view of the public realm.*

All residents' car parking is located at basement level and therefore screened from view of the public realm.

- *Maximum of two crossovers from Road 4.*

The proposed development consists of a single crossover to Road 4 (Milyarm Rise).

- *Signage shall be of a high quality graphic design, simple in format and appropriately located and integrated with the building design and landscape to minimise a cluttered appearance. Entry communication and mail delivery boxes shall be discreet and integral to the building design.*

Any signage associated with the development will be of a high quality and located and integrated appropriately within the building design to ensure an attractive and uncluttered appearance. An internal mail room is integrated into the building design.

- Entrances shall be well lit for safe use for after dark.

All entrances will be lit appropriately for safe use at night.

- A screened, central waste storage and collection space shall be included to accommodate bins (in accordance with City of Nedlands' standard waste and recycling system requirements).

A screened bin storage and collection area is located centrally at ground floor adjacent to the vehicle access point. Waste storage and collection spaces will be provided generally in accordance with the City of Nedlands' standard waste and recycling system requirements. Further details in regard to waste management are contained within the waste management plan prepared for the proposed development and lodged with this development application.

In light of the above, it is clear the proposed development complies with all of the 'other requirements' of the Design Guidelines for Precinct 7 and warrants approval accordingly.

5.3.2 Environmental performance

The Design Guidelines require development to be designed to a certain level of environmental performance to improve energy efficiency and minimise heating and cooling costs. The Design Guidelines specify mandatory and recommended environmental performance features for Precinct 7.

Mandatory items

While the majority of mandatory environmental performance items are detailed design features to be addressed at the building licence stage, solar access is one item that is impacted by the design, layout and orientation of the building and therefore must be considered at the planning stage.

It is a mandatory requirement of the Design Guidelines that development at Precinct 7 have:

Access of summer sun into openings and outdoor living areas shall be controllable (eg. through the use of full height and moveable screens).

The proposed design incorporates screens throughout the building to control sun penetration into openings and outdoor living areas. As such, the proposed development satisfies the mandatory requirements of the Design Guidelines to be considered at the planning stage.

Recommended items

The following environmental performance features are also recommended to be incorporated into the design for development of Precinct 7:

- Capturing of cooling breezes (cross ventilation) to minimise use of mechanical cooling systems.

- Minimising daytime living areas and single aspect apartments with an orientation and openings to the south only to encourage use of natural heating through solar gain in winter.
- A suitably sized underground water tank shall be installed as a back up for the purpose of reticulating landscaped areas and will augment the standard irrigation system.

The proposed development is designed around a central void to maximise cross ventilation through units and increase northern sun exposure for dwellings at the southern end of the development. Natural ventilation to common corridors will also be provided to minimise need for mechanical cooling. A central gas hot water system with solar boosted technology is proposed. Landscaping will be designed to have low watering requirements. The environmental performance features proposed are therefore generally in accordance with the recommended features, and will contribute to achieve the objectives for a high level of environmental performance for the development overall.

5.3.3 Built form and landscape

The Design Guidelines also contain a number of built form and landscape development standards applicable to all precincts. The relevant standards are addressed as follows.

Architectural character and integrity

The Design Guidelines require a distinctly contemporary and high quality building character with attention to detail. Building design is to reflect modern lifestyles, neighbourly co-existence and site conditions. Design shall be original, robust and environmentally responsive. Imported or historical architectural colours and styles are not supported. Contextual aspects to be considered include:

- parks and streetscape
- surrounding existing development and land use
- topography
- microclimate
- views and vistas (subject to conditions – refer to section on Site Planning)
- orientation and ability to provide "eyes on the street" for passive surveillance of the public realm
- access by pedestrians and vehicles.

The proposed development offers a contemporary design that demonstrates originality in responding to the unconventional lot shape it is situated on. The design incorporates detailed facades that vary high quality materials to create interest and amenity. The proposal is responsive to the environment in the way it works with the existing sloping topography of the subject site, in that its design with a central void increases ventilation and solar access to dwellings for passive heating and cooling. The proposed development addresses all street and park frontages to provide a high level of passive surveillance. It also capitalises on the views and vistas afforded by the subject site, and proposes appropriate vehicle and pedestrian access routes. Accordingly, the proposed development is deemed-to-comply with the architectural character and integrity requirements of the Design Guidelines and should be approved as such.

Elevations

The Design Guidelines require architectural excellence that responds appropriately to the public realm. Elevations to street frontages and Public Open Space shall include major openings, habitable rooms and their associated activity. Rear elevations and any visible walls built to the boundary shall match the quality of street facing primary elevations. Detailing and feature elements such as balconies, porches, openings, shade devices and clearly defined and sheltered entrances should be used to create expression, interest and identity in elevations. Large areas of blank unarticulated wall visible from the street are not supported. Curved, dark and reflective glass shall not be used in openings where visible from the street.

The proposed development contains high quality articulated elevations to both primary and secondary street frontages, and neighbouring Public Open Space and school playing fields. The inclusion of major openings, balconies, varied materials and colours and detailed features in design afford activity and surveillance around the entire site and create an attractive and interesting development. The proposal contains no expansive blank walls and will use clear glass to all openings. The elevations as proposed clearly comply to the requirements of the Design Guidelines and warrant approval accordingly.

Roofscape

The Design Guidelines require consideration of integrated roof, eaves and elevations. They further require that flat rooves to have fascia edging detail or be screened with parapet walls, and roof colouring shall be light to reduce heat absorption.

The proposed design incorporates a flat roof that is integrated with fascia appropriately to provide for an aesthetically pleasing elevation. Roof materials will be coloured to minimise heat absorption. Additionally, sky gardens located on the rooftop will provide a measure to further mitigate heat absorption. As such, the proposed development can be deemed-to-comply to the requirements of the Design Guidelines in regard to roofscape.

Materials and colours

Colours and materials that convey a contemporary aesthetic are supported by the Design Guidelines. Bold use of a variety of quality materials and colours is supported to achieve architectural richness. Recommended materials include:

- *rendered masonry*
- *feature stone and limestone*
- *small areas of feature face brickwork*
- *recycled, plantation or sustainable harvested timber*
- *metal elements and details.*

The proposed development contains rendered and painted masonry, feature stone cladding, 'timber like' battening and sunscreens, aluminium and glass balustrades, among other detail features to create a high quality presentation that is interesting and diverse. These features provide for a built form that is accordingly considered consistent with the materials and colours requirements of the Design Guidelines.

Fences

The Design Guidelines specify for Precinct 7 any fencing facing to the street front shall not be higher than 1.8 metres. This measurement shall exclude any retaining wall the fences may be built upon. In regard to retaining walls, new retaining walls shall be constructed in the same material and colours as the existing retaining walls constructed by Mirvac. Any alternate design will have to be considered for approval.

Fences to street front on the proposed development do not exceed 1.2 metres above the ground floor level. Retaining walls are proposed in areas where natural ground levels drop. The proposed retaining walls will be constructed to Mirvac's specifications, or approval for any alterations will be sought. Accordingly, the proposed development can be deemed-to-comply with the fencing requirements of the Design Guidelines and should be approved accordingly.

Landscape

The Design Guidelines state careful consideration should go towards achieving landscaping and open space that minimises water use and includes appropriate structures or plants to provide shade in summer and solar access in winter. The Design Guidelines contain a list of recommended water wise plant species for landscaping. The lot owner shall provide reticulation to the adjoin verge as part of its landscaping treatment and will be responsible for maintenance of the verge.

Landscaping provided for the subject site will appropriately consider water usage, shade provision and solar access, using species from the recommended list. It will include landscaping and reticulation of the adjacent verge. The proposal can be considered generally in accordance with the landscaping requirements of the Design Guidelines.

5.3.4 Services

Finally, the Design Guidelines contain specific provisions to ensure essential services do not detract from the amenity of the development.

Services

The Design Guidelines specify:

- *All pipes, wired services, clothes drying areas, hot water storage tanks and such items shall be concealed on the Lot, and shall not be seen from anywhere in the public realm.*
- *Meter boxes shall be located in the least obvious location from public view.*
- *TV antennae, satellite dishes and radio masts shall not be seen from the primary or secondary streets. These items shall be located where they are in the least obtrusive location from the neighbour's property (for example, located at the rear of the roof and below the roof ridge level away from the neighbour's outdoor living area).*
- *Roof and wall mounted air conditioning units are not permitted unless fully concealed.*

The proposed development conceals from public view and appropriately locates all services as required and should be approved accordingly.

Stormwater

The Design Guidelines require all stormwater to be collected within the lot.

The proposed development will collect all stormwater onsite, and should be approved accordingly.

Waste and recycling

In regard to waste and recycling, the Design Guidelines require as follows:

- *Waste bin/s shall be stored and concealed from public view on the Lot.*
- *Provision shall be made for the appropriate number of bin pads (in accordance with City of Nedlands' standard waste and recycling system requirements).*

The proposed development provides for a bin store room to store and conceal bins from public view. Bins will be provided in accordance with City of Nedlands' standard waste management and recycling requirements. Further details in regard to waste management are contained within the waste management plan prepared for the proposed development and lodged with this development application.

Storage

The Design Guidelines state:

- *Commercial vehicle, including caravans, boats, trailers etc. shall not be parked or stored on the Lot unless enclosed within a garage or fully screened from public view.*

The proposed development does not contain areas for storage of commercial vehicles or trailers of any sort and residents would not be expected to park or store such vehicles on the subject site. Any vehicle parked in the residents' allocated basement parking bays will be fully screened from public view. As such, the proposal complies with the Design Guidelines' requirements for storage and should be approved accordingly.

5.4 State Planning Policy 3.1 – Residential Design Codes

The proposed development has been assessed against the new version of Planning Policy 3.1 – Residential Design Codes (**R-Codes**), which are to come into effect 2 August 2013. It is considered, while the new R-Codes have not yet been formally Gazetted, the subject development application will be assessed over the upcoming period of transition and will be determined and built when the new R-Codes are in effect.

Part 6 of the R-Codes pertains to design elements for multiple dwellings in areas with a coding of R30 or greater. The following section discusses the design elements pertinent to the proposal.

5.4.1 Street surveillance

Design element 6.2.1 addresses street surveillance. The deemed-to-comply requirements of the R-Codes for street surveillance are:

- C1.1 The street elevation(s) of the building to address the street with facades generally parallel to the street and with clearly definable entry points visible and accessed from the street.*
- C1.2 The building has habitable room windows or balconies that face the street.*
- C1.3 Basement parking structures between a street frontage and the main front elevation are no more than 1m above natural ground level at any point.*

The proposed development addresses both Milyarn Rise and Alfred Road with facades that generally follow the alignment of the street. Entrance points are clearly defined and accessed from Milyarn Rise. Habitable rooms and balconies front to both Milyarn Rise and Alfred Road. Therefore, the proposed development is deemed-to-comply with C1.1 and C1.2 in respect to street surveillance.

Basement parking rises about 2 metres above ground level around the middle of the western elevation (Milyarn Rise), at the northern corner of the western elevation and at the eastern corner of the northern elevation (Alfred Road). The protrusion of the basement parking more than 1 metre above ground level is the result of the varying topography of the subject site, and the elevation of the basement above ground level varies to as low as nil along both street frontages. Thus, the existing topography of the subject site presents a unique context, and this aspect of the street surveillance requirements of the R-Codes should be considered under the design principles for street surveillance.

The applicable design principle for street surveillance is as follows:

- P1.1 Buildings designed to provide for surveillance (actual or perceived) between individual dwellings and the street and between common areas and the street, which minimise opportunities for concealment and entrapment.*

The proposed development has a large number of dwellings overlooking each street frontage that can be expected to minimise concealment opportunities. Additionally, the situation where dwellings on upper levels will look down towards the street will provide surveillance over the select areas where basement walls rise up to 2 metres above ground level. In light of the above, it is considered the proposed development incorporates sufficient street surveillance to comply with the design principle of the R-Codes and should be approved accordingly.

5.4.2 Sight lines

Design element 6.2.3 of the R-Codes discusses sight lines at vehicle access points. The deemed-to-comply requirement states:

- C3 Walls, fences and other structures truncated or reduced to no higher than 0.75m within 1.5m of where walls, fences, other structures adjoin vehicle access points where a driveway meets a public street and where two streets intersect, (refer to Figure Series 9).*

The proposed design includes walls to the vehicle driveway that extend to the property boundary. These walls stand between approximately 0.4 and 0.6 metres in height dependent on ground level. As such, no wall measures higher than 0.75 metres within 1.5 metres of where the driveway meets the street and the proposed development can be deemed-to-comply with the R-Codes in respect to height lines.

5.4.3 Building appearance

Design element 6.2.4 of the R-Codes pertains to building appearance. The deemed-to-comply requirement for building appearance is:

- C4 *Buildings that comply with the provisions of a special control area, with the provisions of a local planning policy made under the scheme or with the provisions of the scheme, in respect of the design of carports and garages, the colour, scale, materials and roof pitch of buildings including outbuildings, the form and materials of retaining walls and the extent to which the upper levels of buildings are viewed from the street should be limited.*

The proposed development complies with the provisions of the Design Guidelines and DAP adopted as a Local Planning Policy under TPS2 in respect to all items relevant to the specific context, including colour, scale, materials, roof pitch and retaining walls. Therefore, the proposed development is deemed-to-comply with the building appearance requirements of the R-Codes and warrants approval as such.

5.4.4 Outdoor living areas

Design element 6.3.1 of the R-Codes addresses outdoor living areas. The deemed-to-comply requirement of the R-Codes for outdoor living areas states:

- C1 *Each unit is to be provided with at least one balcony or equivalent accessed directly from a habitable room with a minimum area of 10m² and a minimum dimension of 2.4m.*

Each dwelling of the proposed development is provided with a balcony accessible from a habitable room. All dwellings have at least one balcony with a minimum dimension of at least 2.4 metres and a minimum area of at least 10 square metres. As such, the proposed development can be deemed-to-comply with the requirements of the R-Codes for outdoor living areas and warrants approval accordingly.

5.4.5 Parking

Design element 6.3.3 of the R-Codes pertains to car and bicycle parking requirements. **Table 3** displays the deemed-to-comply requirement of the R-Codes for on-site car parking as compared to the proposed on-site car parking.

Table 3 – Parking Requirements

Dwelling size	Parking standard	Required car bays	Provided
Small (<75 sqm or 1 bedroom)	1 per dwelling	(95 dwellings) 95 bays	346 Residential bays
Medium (75 – 110 sqm)	1.25 per dwelling	(94 dwellings) 118 bays	
Large (>110m ²)	1.5 per dwelling	(16 dwellings) 24 bays	
Visitors	0.25 per dwelling	(205 dwellings) 52 bays	15 Visitor bays
Total		289 bays	361 bays
Total Surplus			+109 Residential bays
Total Shortfall			-37 Visitor bays

Table 3 demonstrates the proposed development provides for an overall surplus of bays provided as compared to what is required. When broken down, the development proposes a surplus of 110 residential bays and a shortfall of 37 visitor bays. Accordingly, the application seeks a variation to the deemed-to-comply requirement of the R-Codes with regard to visitor parking.

It is considered the visitor parking requirements of Part 6 of the R-Codes significantly overestimate the demand for visitor parking, as they are based on the premise that, at any given time, 1 in 4 dwellings will have a visitor who has driven to the site. It is considered the actual demand for visitor parking, particularly for inner urban multiple dwelling developments, is substantially lower than that envisaged by the R-Codes.

It is submitted the level of car parking provided for by the proposed development is adequate and appropriate to the context. An additional 4 on-street car parking bays are located just south of the subject site on Milyarm Rise. The proposed development is serviced by several adequate quality bicycle routes and Transperth bus routes 381 and 27. The subject site is located approximately 1200 metres from the Swanbourne train station, providing connection to both Perth and Fremantle. Furthermore, with the substantial oversupply of residential bays, it can be expected that some residents may not use both of their car parking bays on a regular basis and will be able to offer visitors the option to park in their second allocated bay when visiting the subject site. Traffic engineers have been engaged to prepare a visitor parking study for mixed-use and multiple dwelling developments across Perth. This study will be submitted to the City to provide further justification in regard to the proposed shortfall to visitor parking requirements.

In respect to bicycles, C3.2 of the R-Codes states the deemed-to-comply requirement for bicycle parking to be one bicycle space for every three dwellings for residents and one bicycle space for every ten dwellings for visitors (provided in accordance with AS2890.3). The proposed development consists of 205 dwellings, and therefore requires 68 bicycle bays for residents and 21 bicycle bays for visitors. The proposed development provides at total of 89 bicycle parking spaces, accommodating the required 68 resident bicycle parking bays and 21 visitor bicycle parking bays. Therefore, the proposed development can be deemed-to-comply with the requirements of the R-Codes for bicycle parking.

5.4.6 Design of car parking spaces

Design element 6.3.4 of the R-Codes addresses the design of car parking spaces. The deemed-to-comply requirements of the R-Codes are as follows:

- C4.1 Car parking spaces and manoeuvring areas designed and provided in accordance with AS2890.1 (as amended).
- C4.2 Visitor car parking spaces:
 - marked and clearly signposted as dedicated for visitor use only, and located close to or visible from the point of entry to the development and outside any security barrier; and
 - provide an accessible path of travel for people with disabilities.
- C4.3 Car parking areas comprising six or more spaces provided with landscaping between each six consecutive external car parking spaces to include shade trees.
- C4.4 All car parking spaces except visitors' car parking spaces fully concealed from the street or public place.

Car parking provided with the proposal is designed in accordance with Australian standards. Visitor parking is clearly marked and accessible, conveniently to the parking garage entrance on the upper basement level 1. All visitor parking bays will be signposted as such. No car parking bays will be located externally, and therefore landscaping is not necessary. All parking is concealed within the enclosed basement. As such, the proposed development has designed car parking spaces in accordance with the deemed-to-comply requirements of the R-Codes and warrants approval accordingly.

5.4.7 Site works

Design element 6.3.6 of the R-Codes pertains to site works. The deemed-to-comply requirements of the R-Codes with respect to site works are as follows:

- C6.1 Excavation or filling between the street and building, or within 3m of the street alignment, whichever is the lesser, shall not exceed 0.5m, except where necessary to provide for pedestrian or vehicle access, drainage works or natural light for a dwelling.
- C6.2 Excavation or filling within a site and behind a street setback line limited by compliance with building height limits and building setback requirements.
- C6.3 Subject to clause 6.3.6 C6.2, all excavation or filling behind a street setback line and within 1m of a lot boundary shall not be more than 0.5m above the natural ground level at the lot boundary except where otherwise stated in a local planning policy or local development plan.

The proposed development is designed to integrate with the existing ground levels of the lot created at the time of subdivision. As such, the proposed development does not require any excavation or filling and can be deemed-to-comply with the requirements of the R-Codes in respect to site works.

5.4.8 Visual privacy

Design element 6.4.1 of the R-Codes addresses visual privacy. The deemed-to-comply requirement for visual privacy in areas coded higher than R50 is major openings and unenclosed outdoor active habitable spaces, with a floor level more than 0.5 metres above natural ground level, overlooking any other residential property behind its street setback to be set back from the lot boundary a minimum of:

- 3 metres for major openings to bedrooms and studies;

- 4.5 metres for major openings to habitable rooms other than bedrooms and studies; and
- 6 metres for unenclosed outdoor active habitable spaces.

Alternately, development shall be provided with 75 per cent obscure permanent screening to at least 1.65 metres to restrict overlooking to adjoining properties.

The proposed development does not neighbour to any residential properties. Within the subject site, individual dwellings are distanced adequately to avoid any visual privacy issues. Where balconies adjoin each other, solid walls will provide screening. Accordingly, the proposed development is deemed-to-comply with the requirements of the R-Codes in respect to visual privacy and warrants approval.

5.4.9 Solar access for adjoining sites

Design element 6.4.2 pertains to solar access for adjoining sites. The deemed-to-comply requirements of the R-Codes in regard to solar access aim to ensure that no development casts an unreasonable shadow over a neighbouring residential property, solar collector, or north facing major openings to habitable rooms. The subject site abuts to POS and primary school playing fields. Therefore, the proposed development does not cause any overshadowing issues and satisfies the deemed-to-comply requirements of the R-Codes.

5.4.10 Dwelling size

Design element 6.4.3 of the R-Codes pertains to dwelling size. The deemed-to-comply requirements with regard to dwelling size state the following:

- C3.1 Development that contains more than 12 dwellings are to provide diversity in unit types and sizes as follows:
 - minimum 20 per cent 1 bedroom dwellings, up to a maximum of 50 per cent of the development; and
 - minimum of 40 per cent 2 bedroom dwellings;
- C3.2 The development does not contain any dwelling smaller than 40m² plot ratio area.

The proposed development consists of 205 dwellings in total, 40% of which are single bedroom dwellings and 50% of which are 2 bedroom dwellings. The smallest dwelling in the proposed development is 40 square metres in plot ratio area. As such, the proposed development meets the deemed-to-comply requirements of the R-Codes in respect to dwelling size and should be approved accordingly.

5.4.11 Utilities and facilities

Design element 6.4.6 of the R-Codes discusses utilities and facilities. The deemed-to-comply requirements for utilities and facilities are as follows:

- C6.1 An enclosed, lockable storage area, constructed in a design and material matching the building/dwelling where visible from the street, accessible from outside the dwelling,

with a minimum dimension of 1.5m and an internal area of at least 4m² shall be provided for each multiple dwelling.

C6.2 Where rubbish bins are not collected from the street immediately adjoining a dwelling, there shall be provision of a communal pick-up area or areas which are:

- i. Conveniently located for rubbish and recycling pick-up;*
- ii. Accessible to residents;*
- iii. Adequate in area to store all rubbish bins; and*
- iv. Fully screened from view from the primary or secondary street.*

C6.3 Clothes-drying areas screened from view from the primary or secondary street.

The proposed development contains a communal-bin area in accordance with C6.2 and will provide appropriate clothes drying areas screened from view as required by C6.3 of the R-Codes at the detailed design stage.

The proposed development provides for 205 enclosed, lockable storage areas, all of a minimum area of 4 square metres. In respect to minimum dimension, 13 of the 205 stores have a minimum dimension slightly less than 1.5 metres as a result of the irregular shape of the building producing angled walls. For these stores the majority of the dimension is 1.5 metres and the total area remains to be at least 4 square metres. For these reasons, it is considered that the stores as proposed adequately satisfy the design principle of the R-Codes in respect to utilities and facilities, which requires storerooms convenient to residents, screened from view and able to be secured and managed.

6 Conclusion

In light of the above, we consider the proposed development represents an opportunity to increase dwelling diversity and residential density through targeted infill of a key strategic site in a central metropolitan area. The proposal will contribute positively to the character of the streetscape and surrounding public spaces. The proposed built form is a high quality design that utilises variation in materials, setbacks, colours and textures, as well as complementary landscaping to create an attractive presentation to all of the developments elevations.

In summary, as demonstrated by this report, the development:

1. Is consistent with the provisions of the Metropolitan Region Scheme.
2. Is consistent with the provisions of the City of Nedlands Town Planning Scheme No. 2.
3. Satisfies the development requirements and objectives of the Swanbourne Design Guidelines and Detailed Area Plans.
4. Satisfies the deemed-to-comply requirements and design principles of the Residential Design Codes.
5. Achieves the objectives of Directions 2031 and the Sub-Regional Strategy, which promote infill development of key strategic sites in well serviced central metropolitan areas.
6. Will contribute positively to the character and amenity of the locality.

In light of the above, we request the City's officers refer the application to the Development Assessment Panel with a recommendation of support, following conclusion of required public consultation.

The officer's technical comments provided below are a brief response to the issues raised during consultation. For detailed comments regarding a number of the issues raised during consultation (i.e. building height and plot ratio), refer to the Discussion section of the responsible authority report.

Issue	Officer's Technical Comments
<p>Building Height:</p> <ul style="list-style-type: none"> • Height will affect the amenity of the area • Too big with significant negative/aesthetic/overlooking issues • 40% taller than what the requirements are • The heights proposed exceeds the regulations, aesthetically this is a concern • Will have an adverse effect on the amenity of the neighbourhood • Height will impede as backing onto a school • Height will be imposing, block views and affect privacy • The proposed high rise (7 storeys) is not in the keeping with the area – affect the amenity of the area • Building height – portion of the proposed development exceeds the maximum building height limit under 5.3.11(b) of TPS2. TPS2 does not provide any discretion in building height controls, therefore incapable of approval • This lot already has increased height limit allowed under TPS2 which is substantially over the allowed height limit set out in the Rcodes • The extra height request is totally unjustifiable & will have an adverse impact on the amenity of the streetscape • Boundary height is ignored • Does not suit the existing housing within the area • Spoil the adjacent oval – overshadowing concerns • See no reason why height guidelines should be breached, particularly as any structure adhering to the guidelines would already have a substantial visual impact on the amenity of the area • increase by 8m is unacceptable (effectively proposes two extra floors in proposal) • Additional height would result in significant overlooking & overshadowing problems as well as the invasion of privacy for residents and primary school 	<p>The City's Town Planning Scheme No.2 states that for Lot 60 (the subject site) a height restriction of 21m shall apply. However it does not indicate the method of how building height is to be measured.</p> <p>Several legal interpretations have been received regarding the measurement of building height for the subject site. The City has sought legal advice which established that there are at least two arguable methods to calculate building height. These methods are discussed in the report. The assessment of the building height using both of the outlined methods results in the building exceeding the 21m height restriction.</p> <p>The City considers that the 21m height restriction is mandatory and there is no discretion for this to be varied. As it is believed that the building height is incapable of approval, it is recommended that the application is refused on this basis.</p>

<p>Plot Ratio:</p> <ul style="list-style-type: none"> • Significantly exceeds the maximum plot ratio permitted on the site (refer to 5.2.3 of TPS2 & 6.1.1 Rcodes) which equates to 9369sqm • R80 is meant to have 56 apartments, not 205 apartments • Plot ratio should be 1.0 not proposed which is 2.43 • Overcrowding • Plot ratio will mean there is no landscaping • High residential living should not be introduced into Swanbourne • Plot ratio will impede as backing onto a school • Far to high • Noise issues associated with the amount of people • Plot ratio makes a mockery of the Council's own regulations. The plot ratio is not adequate to support so many dwellings • Plot ratio is unacceptable in a residential setting • Totally inconsistent with the R-Codes prescriptions & local area • Grossly out of context with the immediate zoning plan of the area (majority of building forms within the immediate environment are essentially single residential in nature) • Totally out keeping with the zoning • Proposed increase is extraordinary & appears to have little planning justification, and offers nothing in the way of tangible outcomes for the community that would be expected 	<p>There are inconsistencies between the Residential Design Codes and the Design Guidelines in relation to plot ratio. However it established that the Residential Design Codes prevails and that the deemed-to-comply provision requires a maximum plot ratio of 1.0.</p> <p>The development proposes a plot ratio which is 2.43 times larger than the plot ratio sets out in the deemed-to-comply provision of the Residential Design Codes. Nonetheless, where the deemed-to-comply provision is not met, the plot ratio is then considered against the design principle. Therefore, plot ratio is a discretionary element. The design principle requires assessment in relation to the bulk and scale indicated in the local planning framework with regard to the existing or future desired built form of the locality.</p> <p>It is acknowledged that there is an argument that as the design guidelines does not require a maximum plot ratio, this satisfies the design principle. In assessing the proposed plot ratio against the design principle, the City deems that when collectively examining the development controls which influence the bulk and scale, it is considered that both the increased building height and the additional minor incursions (including large areas of balconies and other architectural features) the development does not meet the bulk and scale indicated in the local planning framework nor the desired built form of the locality.</p> <p>As it is considered that the development does not satisfy the applicable design principle, the City recommends that the proposed plot ratio is refused on this basis.</p>
<p>Setbacks:</p> <ul style="list-style-type: none"> • They are not compliant • Setbacks – especially basement of only 0.6m than 4m • Minimum setbacks are not met causing crowded frontage which will mean less parking space • Need the setbacks, otherwise will be an 'in your face box' dominating the neighbourhood • The proposal is inconsistent with the existing localities setbacks • Boundary setback is ignored 	<p>Following the submission period, the applicant revised the plans and amended the basement and ground floor levels so the setbacks complied. The setbacks variation proposed are on the west (Milyarm Rise) and north (Alfred Road) elevations and are considered to be minor. Refer to the report for further discussion.</p> <p>The minimum building setbacks are specific to the site and are as per the design guidelines. It is noted that the setbacks in the Residential Design Codes do not apply in this case.</p>

<ul style="list-style-type: none"> • Requirement of appropriate setbacks of at least 10m in case of firebreaks • Overwhelming scale • Not in keeping with the low scale residential environment surrounding the development site • Reduction in balcony setbacks should not be permitted 	
<p>Bulk and Scale:</p> <ul style="list-style-type: none"> • Development is too large • Significant scale of the development • Will have an adverse effect on the amenity of the neighbourhood • A development of this magnitude would interfere with the general aesthetic and community feel • The bulk and scale of the proposed development is not considered to be consistent with the local planning framework due to the maximum building height and building setback provisions • Massive residential block on scale and will ruin the amenity of the area • The proposal is inconsistent with the existing localities bulk and scale 	<p>The City considers that the building exceeds the intended bulk and scale for development at the site.</p> <p>The Residential Design Codes note that the building envelope which is defined by the development controls of height, setback, open space and plot ratio; assists in determining whether the overall scale and mass of a development is consistent with the existing or desired character.</p> <p>As the increased building height and the additional minor incursions (including large areas of balconies and other architectural features) exceeds the building envelope, it is deemed that the bulk and scale proposed is not appropriate.</p>
<p>Amenity:</p> <ul style="list-style-type: none"> • Will completely affect the amenity and character of the Swanbourne area • Swanbourne deserves respectful and sympathetic development, rather than high impact • This is against the whole image the area was initially trying to promote • Doesn't blend in with the surrounding streets • 205 apartments will equate to over 500 people which will have a detrimental effect on surrounding quality of life • Affect the amenity, particularly next to a school • Property is opposite development – intersection with the street etc will amenity of future residents • Adversely affect the amenity of the surrounding area for the following reasons (refer to 5.5.1 TPS2): <ul style="list-style-type: none"> - Increased traffic on Milyarm Rise - Reduced safety and efficiency at the intersection of Milyarm & Alfred Road - Lack of visitor parking and subsequent impact on traffic congestion - Above height fencing & elevated basement parking 	<p>The proposed multiple dwelling development is a permitted activity on the site.</p> <p>The development generally complies with the statutory framework for the site, with the exception of building height, building size (plot ratio) and minor incursions (i.e. balconies) into the setback area. Where these elements do not comply it is considered that the amenity of the area will be affected.</p>

<ul style="list-style-type: none"> - Bulk & scale by significant plot ratio variance & setbacks • Impact on views of City skyline (already established on Nidjalla Loop, Birrigon Loop & Wongin Way) • Will have adverse impact on existing residents in the area • totally disregards the intention to & spirit of a planned community • It will destroy the whole nice, peaceful environment 	
<p>Development Location:</p> <ul style="list-style-type: none"> • Development needs to be close to public transport – Claremont Quarter doesn't have this many apartments • This is a suburban Swanbourne, not a transit-oriented infill area like Subiaco or Claremont • Not suitable so close to a Primary School and Residential area • This development is more suited to Multiple dwellings within a mixed use development and activity centres which this site is not • Development site is reasonable isolated in terms of public transport as building of the size proposed are usually located near train or very frequent bus routes • Suited to CBD and areas that can accommodate, this is a well established low density residential area & is completely out of character • Will be much bigger than Claremont quarter – how can this idea come up in a quiet area in the middle of low density 	<p>It is accepted that the development is not located in an activity centre recognised in Directions 2031 such as Claremont or Fremantle.</p>

<p>Traffic:</p> <ul style="list-style-type: none"> • High residential development will impact on traffic congestion for Nidjalla Loop, Alfred Road & Narla Road • High traffic congestion in Narla & Alfred Road, particularly with the close proximity to the school & during peak hours • Swanbourne Primary School traffic causes considerable congestion already, fear this will be exacerbated by development • Development will exacerbate existing congestion issues • Impact on the accessibility of Milyarm Rise & Alfred Road • Nidjalla Loop & Alfred Road are already used as a short cut for non-residential traffic and this will exemplify with development • Safety concerns with increased traffic • Queuing in lengths at the intersection of Milyarm Rise & Alfred Road – not consistent with the Traffic Impact Statement • Concerns of traffic congestion – Alfred Road is untenable from Milyarm Rise • Creating a visual barrier for traffic negotiating entrance to Alfred Rd, and potentially creating obstructions • Traffic flow & traffic management would be prejudicial to the locality and presents out of scale dangers for vehicular, cyclists & pedestrian access and use of Alfred Road • Local roads cannot support the additional traffic • Current road infrastructure is insufficient to cope with high density living (e.g. Alfred Road) • This site is not particularly well serviced by public transports – places additional pressure • Milyarm Rise is a ‘no thru hammer head cul de sac’ design & is not safe manoeuvring of vehicles • Sight lines for vehicles exiting Milyarm Rise and travelling in an easterly direction along Alfred Road are currently poor. • Feel it would be a huge mistake & danger of infrastructure that is already inadequate • Concerns with the large amount of people in one complex with one way entry 	<p>The issue of traffic in relation to the development has been extensively reviewed with the applicant commissioning a traffic assessment, the City appointing an independent traffic consultant to review the provided traffic assessment and review by the City’s Technical Services. All three assessments have concurred that the road network will adequately cater for the additional traffic. Refer to the traffic section of the report for further discussion.</p> <p>It is recommended that for traffic flow and vehicle safety reasons, the intersection of Milyarm Rise and Alfred Road is to be modified. As a result the applicant has made several amendments to the plans including the agreement to amend the road layout as recommended by the traffic consultants. If the application is approved, it is recommended that an applicable condition is included.</p>
<p>Parking Including Reduced Number of Visitor Bays:</p> <ul style="list-style-type: none"> • Car parking does not comply with AS in relation to car parking spaces and manoeuvrability – 	<p>In response to the City’s comments and the submissions received the applicant revised the</p>

<p>shortfall of 37 on site visitor parking bays</p> <ul style="list-style-type: none"> • Currently only four (4) street parking bays on Milyarm Rise & limited opportunities for additional on-street bays • Lack of parking being provided in incomprehensible and beyond belief & totally contrary to the planning requirements for a residential subdivision • Insufficient parking and visitor bays for 205 apartments • Visitor car parking bays are only 30% of the City's requirements • Already insufficient car parking bays in Swanbourne • Overflow parking onto surrounding streets • Availability of car parks • 15 car bays is not adequate for the amount of apartments • The reduction in the allowed visitor car bays is totally unacceptable • Insufficient visitor parking bays, particularly with proposed cafe & boardroom that are not residential • Insufficient visitor parking bays in untenable in a cul-de-sac • Visitors usually expect to be able to park within 100m and as Alfred Road is not a possibility, visitor parking would flow into surrounding streets upsetting neighbours 	<p>plans. Relating to parking, the revisions made were:</p> <ul style="list-style-type: none"> • Changes to the car parking configuration to be in compliance with the Australian Standards. • Provision of 10 extra car bays on site and the developer agreeing to fund 8 on-street car bays opposite the development. <p>The City considers that the proposed number of visitor car bays provided is sufficient. This is because there is a surplus of 98 residential car bays for the development. The majority of the units have been allocated two bays however one of the allocated bays could be available for the resident's visitor/s. Also, if the development is approved, there will be 12 on-street car bays on Milyarm Rise in proximity to the site which would cater for people visiting for short periods.</p>
<p>Noise</p> <ul style="list-style-type: none"> • Noise from increased amount of cars which will impact on surrounding streets • Parties being held on balconies 	<p>Any noise generated from the development will be dealt with in accordance with the legislation which governs noise, the <i>Environmental Protection (Noise) Regulations 1997</i>. It is noted that noise from traffic is not covered by this legislation.</p>
<p>Store Dimensions</p> <ul style="list-style-type: none"> • Not met • Insufficient storage space – need at least 1.5m 	<p>The majority of the stores provided comply with the deemed-to-comply provisions. The variation to the deemed-to-comply provision is that the minimum internal dimension of the store is less than 1.5m. Only 14 out of the 204 stores provided have an internal dimension of less than 1.5m. As a result this variation is considered to be minor. The variation occurs due to the irregular shape of the building producing angled walls.</p>
<p>Basement</p> <ul style="list-style-type: none"> • The basement parking/design and elevation above ground level affects the development's interaction with the street at ground level and 	<p>The plans have been revised and the majority of basement 1 is located below ground level. As a result there is no longer a variation to the</p>

will create poor street environment	R-Codes (deemed-to-comply provision).
<p>Pedestrian Safety:</p> <ul style="list-style-type: none"> • Fear for the safety of pedestrians • Safety for children • Strong concerns for families safety while walking, particularly so close to the Primary School • Traffic congestion will cause danger to local residents 	<p>The issue of pedestrian safety was considered with the Traffic/Transport assessment and there were no concerns raised in relation to pedestrian safety.</p> <p>In order to facilitate walking and pedestrian access to the development, the applicant has agreed to fund the extension of the existing footpath on the southern side of Alfred Road to connect to the entries of the development.</p>
<p>Café Use:</p> <ul style="list-style-type: none"> • Cafe is 'not permitted' and will unfairly steal trade from Asquith commercial precincts • Concerns for the cafe land use and impact of clients/parking/noise issues • Land use permissibility – 'restaurant' under the TPS2 which is an non permissible use in a residential zone • Does not provide any extra parking • Affect the amenity of the area due to increased traffic within surrounding streets • Strain on scarce parking availability 	<p>In response to the submissions received in relation to the proposed cafe, the applicant has removed the cafe from the plans. The proposed cafe is no longer a part of the application.</p>
<p>Visual Privacy:</p> <ul style="list-style-type: none"> • Balconies overlook school oval (directly below) • Major overlooking concerns into back garden • Loss of privacy • Apartments overlooking onto other homes 	<p>The application has been assessed against the visual privacy (overlooking) provisions of the R-Codes and complies.</p> <p>The development site is adjoined by reserves and is located considerable distance away from other residential properties. As such direct overlooking of private property will not occur.</p> <p>It is noted that the R-Codes and Liveable Neighbourhoods promote balconies being located adjacent to public areas as it allows for passive surveillance.</p>
<p>Fencing:</p> <ul style="list-style-type: none"> • High & solid fencing is unsightly & imposing spoiling the amenity of the area as well as being too close to the road • Too high • Lack of street surveillance as the fence wall is solid & high • Height of fencing would substantially detract from neighbour's amenity. A 'green' barrier could help reduce visual impact • Height is excessive and obstructive to the 	<p>In light of the submissions received, the applicant has revised plans have been received which reduce the height of the fencing facing Milyarm Rise. This fencing is now compliant.</p>

<p>street reserve facade</p> <ul style="list-style-type: none"> • Visual permeability of fencing does not promote active/engaging streetscapes which allows for passive surveillance 	
<p>Building Design:</p> <ul style="list-style-type: none"> • Unattractive design 	<p>Noted. The Swanbourne Design Committee has considered the building design against the Swanbourne Design Guidelines and has supported the proposed external appearance of the development.</p>
<p>Property Values:</p> <ul style="list-style-type: none"> • Amount of units will detract residential values of existing residents • Increased traffic congestion will adversely affect the property values 	<p>Property values are not a planning consideration.</p>
<p>Environmental Impact:</p> <ul style="list-style-type: none"> • Concerns for the impact it will have on the environment • Fears for the environmental effects of taking down the trees for development • Fears that the height and bulk will impinge on the natural environment/trees and be removed to accommodate the building • Should respect local surroundings and natural environment • Better for the environment to be public open space • Will impede the natural landscape 	<p>The City does not have control over the clearing of vegetation on private property. It is noted that the development site is surrounded by areas designated for reserves. Refer to Figure 1 of the report.</p>
<p>Impacts of Rental Units:</p> <ul style="list-style-type: none"> • Will promote rental dwellings and become similar to Mosman park which has caused problems • Rental properties will change the atmosphere of the area 	<p>The potential types of home ownership in the development is not a planning consideration.</p>
<p>Number of Variations Proposed:</p> <ul style="list-style-type: none"> • Should not be allowed if everything is not in compliance • Extremely concerned for changes to setbacks, fencing, plot ratio, visitor car bays, car park design, store area dimensions, building height level etc • Other non-conforming provisions – should not be varied so that the proposed development is compatible and conforms with the planned quality of the subdivision 	<p>It is acknowledge that the application proposes a number of variations to the Scheme, Local Planning Policy Swanbourne Design Guidelines and does not meet the deemed-to-comply provisions of the R-Codes. However following the public consultation, the applicant has revised the plans to alleviate some of the issues.</p>
<p>Precedent if Development Approved:</p> <ul style="list-style-type: none"> • Too many breaches in regulations/codes – 	<p>The City's officers have recommended that the DAP refuse the application.</p>

<p>should not be encouraged, especially for future developments</p> <ul style="list-style-type: none"> • Fear if approved will allow for more developments in the Coastal Ward be approved of this scale • Variance from the guidelines as proposed would set an unacceptable precedent for future developments in the subdivision as well as being inequitable to all the purchasers who have had to strictly comply with the guidelines 	<p>Notwithstanding this, each development application is considered on a case-by-case basis.</p>
<p>Planned Subdivision Area, Development should be in Compliance with Specific Design Guidelines:</p> <ul style="list-style-type: none"> • Misleading conduct against the purchasers of lots in this subdivision if approved. • Totally out keeping with what was zoned as allowable on that particular lot • Should be converted into original plan of townhouses • Was lead to believe when purchasing that would be converted into townhouses • Should be rejected due to the breach of numerous planning provisions (height, setbacks, incursions, plot ratio etc) • Cannot see justification to demonstrate performance based outcomes • We have recently built in the area and were forced to comply with extremely restrictive covenants, why should big businesses not have to comply like the rest of us? • The development has far too many changes for this to even be passed • When we built our house we never envisaged such a large development would occur around the corner • Inconsistent with the Swanbourne Design Guidelines: <ul style="list-style-type: none"> - exceeds maximum building height permitted on the development - proposes habitable roof space - exceeds maximum building setbacks - fencing exceeds the maximum permitted height • Residential Design Codes: <ul style="list-style-type: none"> - Does not satisfy Deemed-to-Comply provisions in relation to plot ratio, basement parking, car parking design, visitor parking, fencing and stores • Guidelines are there for a reason & have been implemented with great thought by experts, to give locals a degree of security in the design of their suburb, that they are flouted by developers is a great worry 	<p>The site was subdivided and rezoned specifically for multiple dwellings. The development generally complies with the Swanbourne Design Guidelines. Prior to submitting the development application the Swanbourne Design Committee assessed the development plans and supported the proposed building design.</p> <p>The development generally complies with the statutory framework for the site, with the exception of building height, building size (plot ratio) and minor incursions (i.e. balconies) into the setback area. Where these elements do not comply it is agreed that the development should not be approved.</p>

<ul style="list-style-type: none">• Integrity for council if this were approved• Residential zoning should be adhered to by developments• Gross disregard for the planning measures put in place is disconcerting & surprising• Upon buying our lot, Landcorp/Mirvac clearly conveyed that there were strict planning conditions• The presence of strict, high quality development guidelines I placed high value on and believed that it was a planned community• If approved this would be a tantamount to deceptive & misleading conduct against the purchasers of lots in this subdivision	
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Western
Suburbs



Swanbourne

Design Guidelines B

Precinct 6, Lots 52 to 59: R60 single dwellings
Precinct 7, Lot 60: R80 multiple dwellings
Precinct 8, Lot 61: R60 multiple dwellings

20.09.07
Rev H

A residential project by Mirvac and LandCorp

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Signed: _____

Dated: _____

INTRODUCTION

For all development proposed at Precincts 6 to 8 at Swanbourne, these Design Guidelines shall be met and proposals endorsed by Mirvac prior to a development application for a statutory Planning Approval and Building Licence from the City of Nedlands.

These Design Guidelines provide background information and set the design quality for Swanbourne. Information is included on site planning; environmental performance requirements; the design of a high quality built form and landscape that responds to the context; and integration of services.

Residential lot development at Swanbourne has been carefully planned to include sustainability measures and to harmonise with site conditions. Particular features of the development include:

- an elevated setting and walkable street layout
- Lots with opportunities for views of parkland and attractively landscaped streetscapes
- a variety of Lots with associated planning and design requirements to suit the site context and dwelling types.

GENERAL

The following points define the purpose of the Design Guidelines; site management; the documentation required; and the process for building plans to be endorsed by Mirvac prior to lodgement with Council. The steps are clearly outlined and are designed to achieve results within a minimum timeframe and at the least cost.

1 PROJECT OBJECTIVES

The aim of these Design Guidelines is to create a development that exists in harmony with the environment at the Swanbourne site and the surrounding area. These guidelines apply to all the residential Lots in Precincts 6 to 8 at Swanbourne (*refer Appendix 1*).

The purpose of this document is to prescribe comprehensive building design guidelines to promote appropriate design solutions for development at Swanbourne. The Design Guidelines seek to protect the Lot owner's interest and investment, and enhance the design quality of dwellings proposed at this development. Specifically, this document promotes the following objectives:

- development that has regard for the amenity of adjacent Lots and surrounding public areas
- a strong sense of architectural character that is distinctly contemporary and designed with integrity, and reflects different dwelling types and the characteristics of modern lifestyles
- residential development that is responsive to the site and context, incorporates passive solar design, achieves thermal comfort and ventilation, is energy efficient, and promotes sustainable water management practices
- opportunities for passive surveillance of the public realm, yet respecting individual privacy.

2 STATUTORY REQUIREMENTS

The City of Nedlands has adopted these Design Guidelines and Detailed Area Plans (DAPs) as a Local Planning Policy under Town Planning Scheme No.2 and they should be read in conjunction with the Scheme Text and Planning Policies and Residential Design Codes of WA (R-Codes).

In determining any application for planning approval, the Council will utilise these Guidelines and the DAPs as the primary assessment criteria to be exercised in conjunction with the Scheme and Policies and R-Codes. Where these design guidelines are silent, the Scheme and Residential Design Codes come into effect.

3 BUILDING ENDORSEMENT AND STATUTORY PLANNING APPROVAL PROCESS

A Lot owner shall obtain building endorsement from the Swanbourne Design Committee **before** a formal development application for planning approval and building licence application is lodged with the City of Nedlands – further explanation on this process is provided overleaf.

Town Planning Scheme No.2 requires that Planning Approval must be obtained from the City of Nedlands for all development. The usual local government, Building and Health By-Laws remain in force and Licences (Building and Health) must also be obtained, as necessary from the City of Nedlands.

Signed: _____

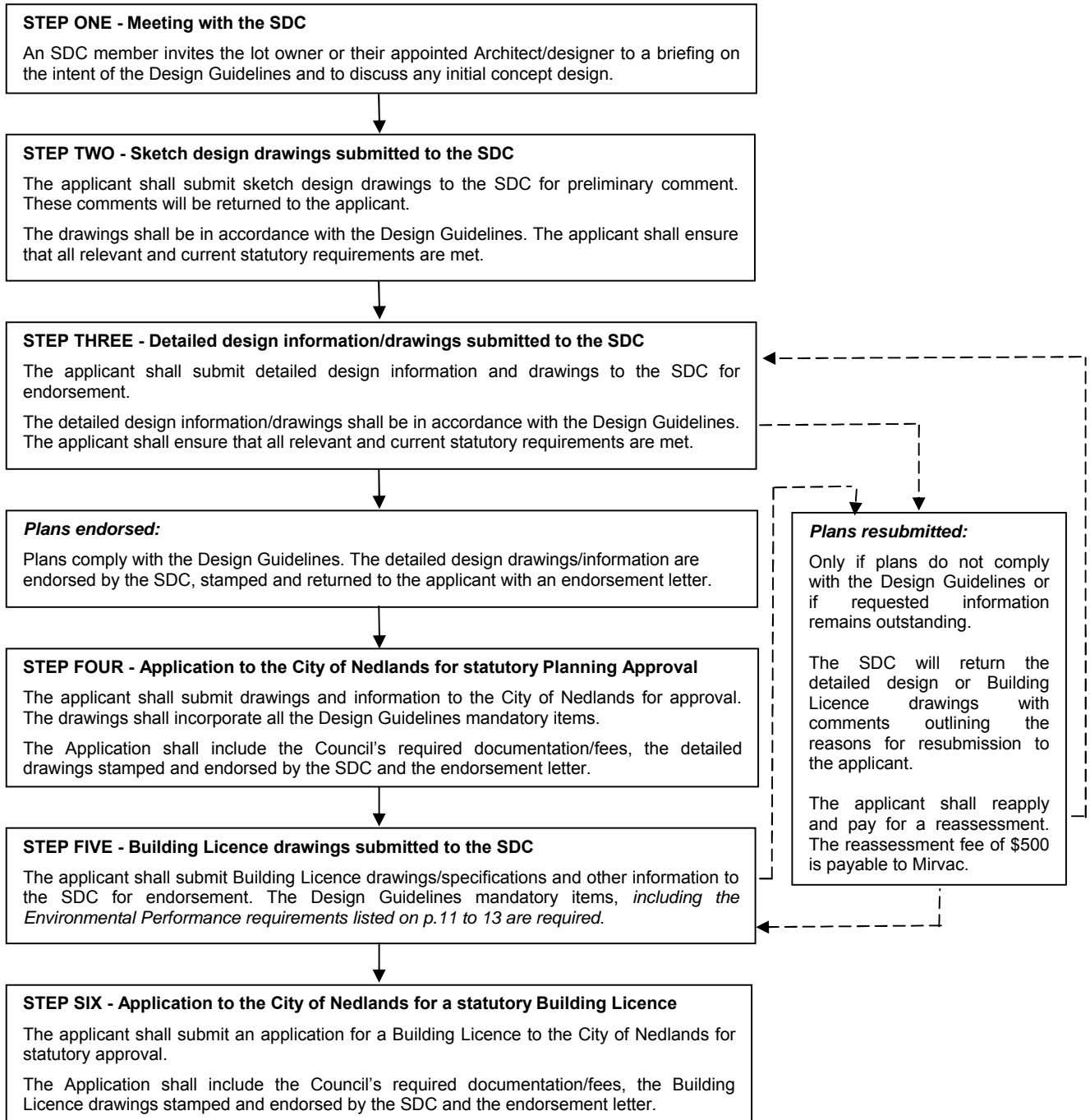
Dated: _____

a) Building endorsement and statutory approval process

The Lot owner shall first obtain endorsement for the building from Mirvac's Swanbourne Design Committee (SDC) for all sketch design drawings and detailed design information prior to a development application for planning approval to the City of Nedlands. The SDC will guide the design development of every dwelling within the contract of sale conditions.

An SDC member will invite the Lot owner or their appointed Architect/Designer to a briefing on the intent of the Design Guidelines and to discuss any initial design concept. An SDC member will be available to answer telephone queries throughout the process.

The SDC's endorsement of a building design shall **not** imply or guarantee a planning approval by the City of Nedlands. The City of Nedlands will be asked to consider the SDC's endorsement of a building design as supporting advice included in the statutory planning process. The flow chart below explains the steps in the building endorsement process prior to a development application for planning approval and building licence application to Council:



A LOT OWNER IS STRONGLY ADVISED TO ENGAGE AN ARCHITECT OR DESIGNER WHO HAS PROVEN SKILLS AND EXPERIENCE IN WORKING WITH DESIGN GUIDELINES, AND DESIGNING AND DOCUMENTING CONTEMPORARY RESIDENTIAL DEVELOPMENT OF A HIGH QUALITY.

Signed: _____
Dated: _____

b) Variations

The Council may approve an application involving departures from the specific criteria of the Guidelines, if in its opinion, the application satisfies the objectives of the Guidelines. The Council may refuse Planning Approval for a development not considered to be in keeping with the objectives of the guidelines.

IMPORTANT:

PRIOR TO SUBMISSION TO COUNCIL, THE APPLICANT SHOULD ENSURE THAT THE APPLICATION SATISFIES THE DESIGN GUIDELINES AND ALL OTHER RELEVANT STATUTORY REQUIREMENTS.

ANY IMPROVEMENTS TO A LOT WHICH ARE NOT IN ACCORDANCE WITH THE APPROVED DEVELOPMENT MUST BE REMOVED UPON NOTIFICATION OR RECTIFIED AT THE EXPENSE OF THE LOT OWNER.

4 DOCUMENTATION

The minimum documentation to be submitted to the SDC's building endorsement process (see *previous section 3*) shall include two paper copies of the following:

a) Step Two: Submission of sketch design drawings

1. Site plan in context, 1:200 min. (showing contextual aspects taken into consideration, existing and proposed critical levels, critical setback and building envelope dimensions, garage location, drying court, screened bin store etc)
2. Floor plans of each level, 1:100 min.
3. Elevations of all sides, 1:100 min.
4. Section showing any changes in level, 1:100 min.
5. Diagram demonstrating any overshadowing of adjoining Lots

b) Step Three: Submission of detailed design information

1. Site plan in context, 1:200 min. (showing existing and proposed critical levels, fully dimensioned in accordance with setbacks and building envelope, garage location, drying court, screened bin storage area etc)
2. Floor plans of each level (including the roof plan), 1:100 min.
3. Coloured elevations of all sides, 1:100 min. (including the position of solar panels and any air conditioning unit)
4. Section showing any changes in level, 1:100 min.
5. Diagram demonstrating any overshadowing of adjoining Lots
6. Materials and Colours Schedule
7. Indicative landscape plan, 1:100

c) Step Five: Submission of Building Licence drawings

1. Building Licence drawings (including compliance with all mandatory Environmental Performance items on p.11 to 13)

All drawings submitted shall include a north point, bar scale, and be fully dimensioned and annotated. The SDC may request any additional information or clarification to support the application.

5 SITE MANAGEMENT AND CONSTRUCTION

It is vital to preserve the amenity of the development prior to, during and after the construction on a dwelling commences. The Lot, therefore, shall be kept clear of excessive weeds and rubbish and maintained to an acceptable standard.

During construction, the Lot owner shall ensure that their builder is responsible for the secure storage of materials and waste on the Lot, and regular removal of any construction waste generated from the Lot.

No building materials, rubbish or other matter should be deposited in the public reserves, in particular the washing of waste from construction vehicles. The verge, footpath and neighbouring property should be kept clear at all times. Any damage to areas outside the Lot shall be rectified upon notification.

Given the compact nature of some of the Lots and the allowance for building up to the side boundary on specific Lots, an adjoining neighbour should provide reasonable access to their property where possible to facilitate development.

Signed: _____

Dated: _____

SITE PLANNING FOR PRECINCTS 6 TO 8

This section outlines the site planning requirements for development on the Lots within Precincts 6 to 8. Where these Design Guidelines are silent, the relevant provisions in the Town Planning Scheme and Residential Design Codes come into effect.

PRECINCT 6: LOTS 52 TO 59 (R60 SINGLE DWELLING)

Lots 52 to 59 are east-west orientated narrow frontage lots.

a) OBJECTIVES FOR DEVELOPMENT

The design for the dwelling on the Lot shall ensure:

- I. an overall terraced appearance from the street front (building walls to both side Lot boundaries) for dwellings on Lots within Precinct 6
- II. a response to the surrounding context of Public Open Space and streetscapes (refer to the section on *Built Form and Landscape*)
- III. that the appearance of a garage opening on the elevation is minimised from the street.

b) DETAILED AREA PLAN

The Detailed Area Plan denotes particular site planning requirements; refer to Appendix 1 – Sheet 1: Precinct 6.

- **Front setback:** 2m min and 4m max
- **Minor horizontal incursion into the front setback:** The maximum depth of intrusion that is permitted is 0.5m and for a maximum of 50% of the site frontage (minor incursions include projections such as a balcony - roofed or unroofed - with no enclosed space underneath, porch, verandah, blade wall or chimney).
- **Side setbacks:** Commencing at the street front setback, at least 25% of the length of both side walls for each storey behind the front setback shall be built up to the side lot boundaries (nil setback); 1m min setback is permitted elsewhere, with R-Codes wall setbacks applicable for any upper level with major openings.

Walls are permitted to be built up to both side boundaries to a height of 12m maximum provided the development's shadow cast at midday, 21 June onto the adjoining lot's outdoor living area does not exceed 50% (refer to DAP for location of outdoor living area).

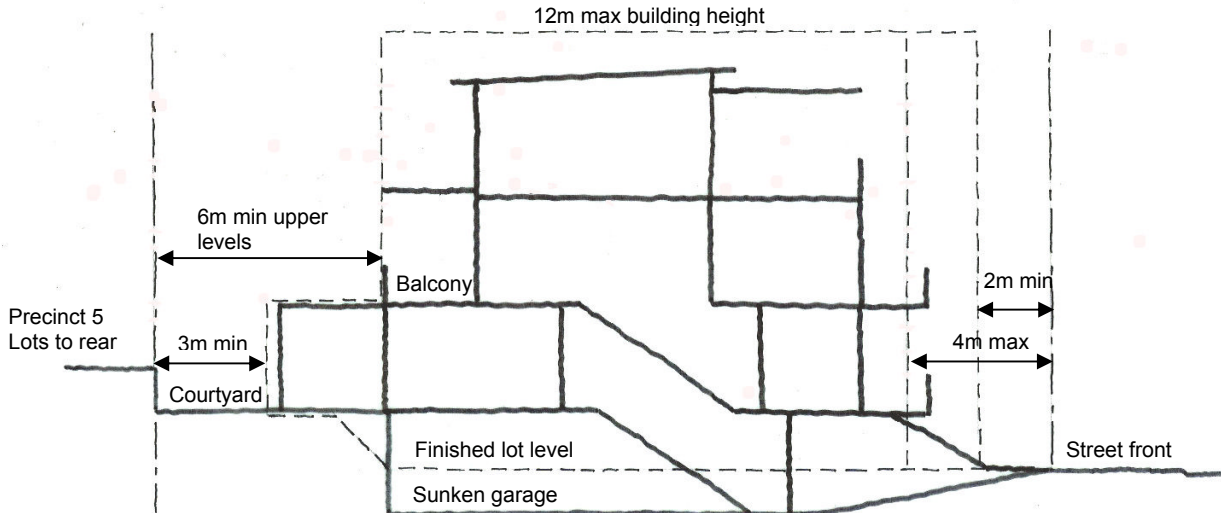
- **Additional side setback provisions for Lots 52 and 59:**
 Lot 52 - All habitable rooms with walls built on the northern boundary shall have at least one 'major' opening. All other rooms with walls built on the northern boundary shall have at least one 'minor' opening.

 Lot 59 - All habitable rooms with walls built on the southern boundary shall have at least one 'major' opening. All other rooms with walls built on the southern boundary shall have at least one 'minor' opening.
- **Rear setbacks:** 3m min at ground level and 6m min for upper levels. A non weather-proof light weight structure is permitted within the rear setback.
- **Privacy:** Any balcony at the upper level that is located at the 6m min rear setback position shall be deemed as complying with the R-Codes privacy provisions. A balcony at the upper level that is set further eastwards into the lot shall be required to comply with R-Codes privacy provisions.
- **Garage setback and design requirements:** 2.5m minimum and located by at least 0.5m behind the dwelling's elevation. A verandah, porch or balcony are included as elements on the "elevation". Any element of the elevation that serves to reduce the visual impact of the garage on the street is deemed to be the dwelling's elevation. The garage may exceed the maximum front setback depth if required.

The enclosed garage shall accommodate a minimum of two car spaces, and shall be integrated into the design of the dwelling.

Signed: _____

Dated: _____



- **Principal outdoor living area:** The courtyard shall be located at the rear western part of the Lot to receive winter sunlight (refer to DAP).
- **Minimum total open space:** 35%
- **Plot ratio:** No plot ratio shall apply.
- **Maximum building height:** 12m calculated from the finished level for each site at the front property boundary established as part of the subdivision works. The height of the building height envelope does not change with the changes in lot levels.
- Habitable loft space and windows in the roof are *not* permitted. A skylight will be considered subject to appropriate detailing including no openings to be permitted.
- Refer to *Built Form and Landscape* section (f) for fence and letter box requirements.

Signed: _____

Dated: _____

PRECINCT 7: LOT 60 (R80 MULTIPLE DWELLINGS)

Lot 60 is a site for multiple dwellings.

a) OBJECTIVES FOR DEVELOPMENT

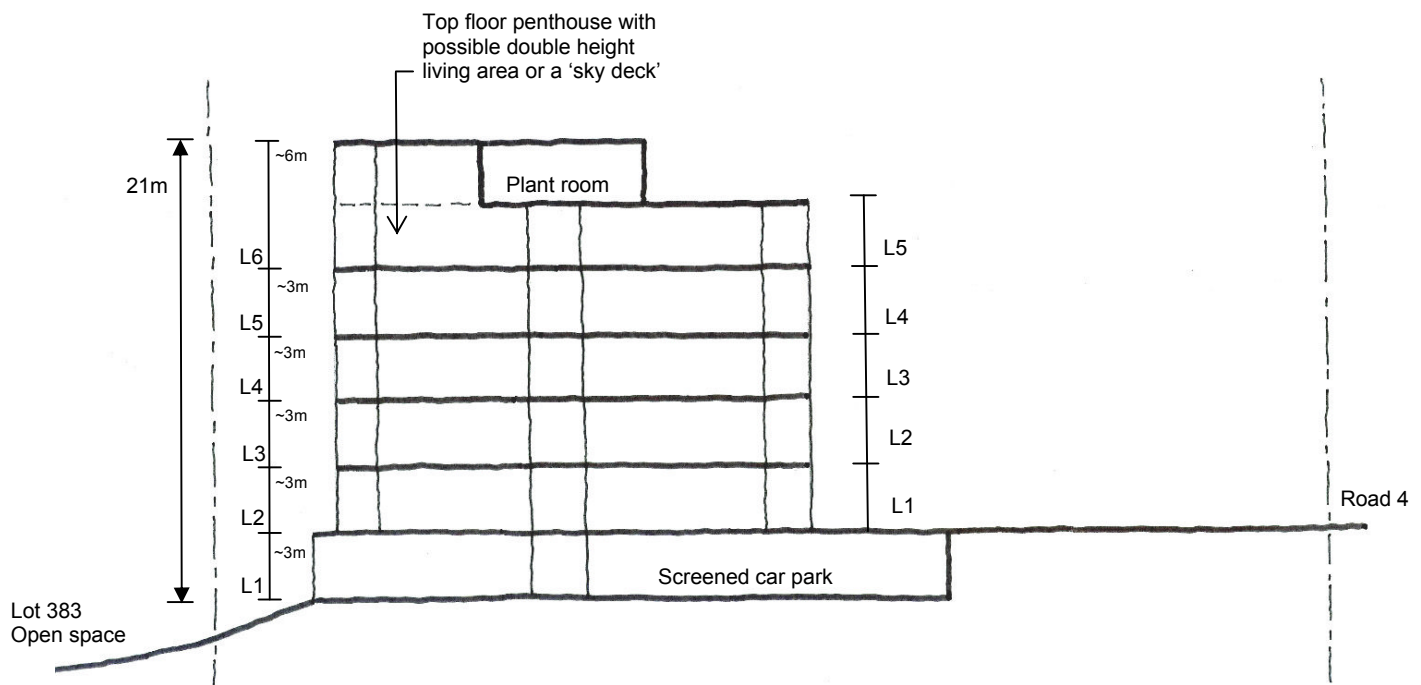
The design for the building on the Lot shall ensure:

- I. a built form that includes articulation of all the elevations. Architectural features such as contrasting materials and colours, awnings, canopies, balconies and bay windows shall provide visual interest
- II. a response to the surrounding context of Public Open Spaces and streetscapes (refer to the section on *Built Form and Landscape*)
- III. passive surveillance opportunities of the surrounding Public Open Spaces.
- IV. a built form that is faceted to follow the curve of the street front boundary and other common boundaries with some variations in building setback.

b) DETAILED AREA PLAN

The Detailed Area Plan denotes particular site planning requirements; refer to Appendix 1 – Sheet 2: Precinct 7.

- **Setbacks:** Refer to the DAP
- **Minor horizontal incursions into the setbacks:** 1m max and maximum of 50% of the elevation (minor incursions include projections such as a balcony with no enclosed space underneath, porch, verandah, blade wall or chimney).

Indicative concept diagram**Indicative concept diagram**

- **Minimum total open space:** 50%
- **Plot ratio:** No plot ratio shall apply.
- **Maximum building height:** 21m and 5 storeys from the street front. The "natural ground level" for measurement of height shall coincide with the finished lot level created at subdivision (refer to DAP).

Signed: _____

Dated: _____

c) OTHER REQUIREMENTS

- Habitable loft space and windows in the roof are *not* permitted. A skylight will be considered subject to appropriate detailing including no openings to be permitted.
- The development shall respond to the surrounding context of streetscapes and Public Open Space by orientating elevations (with habitable rooms, openings and balconies) towards these areas.

A design response that includes large expanses of blank walls and minimal articulation of the elevation and landscape; and any “back of development” appearance (for example, exposed mechanical plant and servicing areas) visible from the public realm will *not* be accepted.

- Residents’ car parking shall be screened on site from view of the public realm.
- Maximum of two crossovers from Road 4.
- Signage shall be of a high quality graphic design, simple in format and appropriately located and integrated with the building design and landscape to minimise a cluttered appearance. Entry communication and mail delivery boxes shall be discreet and integral to the building design.
- Entrances shall be well lit for safe use for after dark.
- A screened, central waste storage and collection space shall be included to accommodate bins (in accordance with City of Nedlands’ standard waste and recycling system requirements).
- Refer to *Built Form and Landscape* section (f) for fence requirements.

Signed: _____

Dated: _____

PRECINCT 8: LOT 61 (R60 MULTIPLE DWELLINGS)

Lot 61 is a site for multiple dwellings.

a) OBJECTIVES FOR DEVELOPMENT

The design for the building on the Lot shall ensure:

- I. a built form that includes articulation of all the elevations. Architectural features such as contrasting materials and colours, awnings, canopies, balconies and bay windows shall provide visual interest
- II. a response to the surrounding context of Public Open Spaces and streetscapes (refer to the section on *Built Form and Landscape*)
- III. passive surveillance opportunities of the Public Open Spaces.

b) DETAILED AREA PLAN

The Detailed Area Plan denotes particular site planning requirements; refer Appendix 1 – Sheet 3: Precinct 8.

- **Setbacks:** Refer to the DAP (note that the utilities easement on the western side of the Lot where development is not permitted is within the setback area).
- **Minor horizontal incursions into the setbacks:** 1m max and maximum of 50% of the elevation of the proposed dwelling (minor incursions include projections such as a balcony - roofed or unroofed - with no enclosed space underneath, porch, verandah, blade wall or chimney).
- **Minimum total open space:** 40%
- **Plot ratio:** No plot ratio shall apply.

c) OTHER REQUIREMENTS

- Habitable loft space and windows in the roof are *not* permitted. A skylight will be considered subject to appropriate detailing including no openings to be permitted.
- The development shall respond to its location abutting northern and southern Public Open Spaces by orientating elevations (with habitable rooms, openings and balconies) towards these areas.
- For the west facing elevation (towards West Coast Highway), openings, other than those with fixed opaque glazing, are not permitted. The west facing elevation shall be articulated however, with architectural richness being encouraged through the bold use of a variety of high quality materials and colours.
- Except as noted for the west facing elevation, a design response that includes large expanses of blank walls and minimal articulation of the elevation and landscape will not be permitted.
- Any “back of development” appearance (for example, exposed mechanical plant and servicing areas) visible from the public realm will *not* be accepted.
- Residents’ car parking shall be screened on site from view of the public realm.
- Signage shall be of a high quality graphic design, simple in format and appropriately located and integrated with the building design and landscape to minimise a cluttered appearance. Entry communication and mail delivery boxes shall be discreet and integral to the building design.
- Entrances shall be well lit for safe use for after dark.
- A screened, central waste storage and collection space shall be included to accommodate bins (in accordance with City of Nedlands’ standard waste and recycling system requirements).
- Refer to *Built Form and Landscape* section (f) for fence requirements.
- A Management Plan shall be submitted that details the operation of building services and any tenancy agreements.

Signed: _____

Dated: _____

ENVIRONMENTAL PERFORMANCE

For Precincts 6 to 8, single dwelling and multiple dwelling development shall be designed to minimise heating and cooling costs; improve upon energy efficiency and minimise greenhouse gas generation. Listed below are mandatory and recommended items for single dwellings and multiple dwelling development.

A - Precinct 6 (Lots 52 to 59): Single dwelling

a) Mandatory items

The mandatory items listed must be shown as part of the design or referenced on an Environmental Performance schedule for assessment and compliance by Mirvac's SDC at Step 3 (Detailed Design dwgs to SDC) and Step 5 (Building Licence dwgs to SDC) as stated, and by the City of Nedlands at Step 4 (Development Application submission) and Step 6 (Building Licence submission) – refer to the process flowchart on p.4.

Item	Theme	Requirement	Assessment stage
M1	Solar Access	A solar hot water system shall be installed to receive sufficient solar gain on a suitable and preferred north facing roof, or a secondary west facing roof. The solar hot water system (with gas booster) shall achieve a minimum of 36 Renewable Energy Certificates. Where the preferred north or secondary west facing roof is visible from the public domain, the storage tank is to be screened from public view (a split system). As far as practicable, the solar panels shall integrate with the roof, be frameless and mounted flush with the roof. All solar collectors, tanks (where permitted) and associated mounting equipment shall be colour coordinated with the roof to minimise adverse visual impacts.	SDC Step 3 and City of Nedlands Step 4
M2		Openings shall be appropriately sized and shaded to reduce solar heat gain in summer.	SDC Step 3 and City of Nedlands Step 6
M3	Energy	The thermal performance of the dwelling should achieve a minimum 6 star Accurate rating, or similar under deemed-to-comply provisions of the Building Code of Australia. The thermal performance of the building envelope shall be maximised through solar access, insulation, shade devices and draught prevention.	SDC Step 5 and City of Nedlands Step 6
M4		At least one bathroom shall include an openable window to the outside.	SDC Step 3 and City of Nedlands Step 6
M5		Any external clothes drying area shall be concealed from public view and well ventilated to dry clothes efficiently.	SDC Step 3 and City of Nedlands Step 6
M6		Any mechanical heating and cooling system shall be sized to match the affected spaces. Any mechanical heating and cooling system shall be minimum 5-star rated.	SDC Step 5 and City of Nedlands Step 6
M7		A gas hob shall be specified.	SDC Step 5 and City of Nedlands Step 6
M8		Natural day light into the dwelling shall be maximised. Glazing, however, shall be minimised on the eastern and western elevations and be appropriately shaded.	SDC Step 3 and City of Nedlands Step 6
M9		Openings shall be located to promote cross ventilation to passively cool the dwelling and reduce reliance on mechanical cooling.	SDC Step 3 and City of Nedlands Step 6
M10		The dwelling's living and sleeping areas shall be capable of being closed off from each other to allow for any localised heating and cooling.	SDC Step 3 and City of Nedlands Step 6

Mandatory list is continued on p.12

Signed: _____

Dated: _____

M11	Water	Water efficient fixtures (for example taps & shower heads) shall achieve a minimum 3 star rating. Toilets shall achieve a minimum 4 star rating.	SDC Step 5 and City of Nedlands Step 6
M12		A rainwater tank of 2500 litres minimum capacity shall be installed to use stormwater, such as from the roof. The tank shall be designed as an integral feature of the dwelling and screened from public view or buried. The tank shall be plumbed for irrigation or toilet use or both.	SDC Step 3 and City of Nedlands Step 6
M13		The required disposal of stormwater on-site shall be made using means such as soakwells, soft paving, segmented and permeable pavers and decking.	SDC Step 5 and City of Nedlands Step 6
M14		Programmable irrigation controllers and tap timers with waterwise irrigation (sub-surface drip irrigation trickle irrigation, drippers, and coarse drop sprays) shall be installed in garden areas.	SDC Step 5 and City of Nedlands Step 6
M15	Materials	Agricultural forestry timbers shall be specified throughout.	SDC Step 5 and City of Nedlands Step 6
M16	Recycling	Suitable provision shall be made internally for waste and recycling - i.e. two bins provided within kitchen cabinetry for recyclables and non-recyclables.	SDC Step 5 and City of Nedlands Step 6
M17	Noise	All mechanical equipment shall be designed and installed to operate at maximum capacity within the Noise requirements of the <i>Environmental Protection (Noise) Regulations 1997</i> .	City of Nedlands investigates any possible noise issue after installation of equipment and upon notification to Council

b) Recommended items

- At least one main living area (for example a lounge, dining or kitchen) and at least one major opening should be located to face north to receive solar heat gain in winter. This space should receive direct sun at 12 noon on 21 June.
- The dwelling should incorporate principles of Universal Design. Key practical initiatives for a 'robust' dwelling design are:
 - At least one level entry accessible from the street
 - Living areas and at least one bedroom and bathroom accessible by a wheelchair user
 - Circulation spaces suitable for a wheelchair user
 - Fixtures and fittings (such as light switches and power outlets) within reach of a wheelchair user
 - Future adaptation possibilities for a dwelling for 'ageing in place'.

Signed: _____

Dated: _____

B - Precinct 7 (Lot 60): Multiple dwellings**a) Mandatory items**

Item	Theme	Requirement	Assessment stage
M1	Solar Access	Access of summer sun into openings and outdoor living areas shall be controllable (eg. through the use of full height and moveable screens).	SDC Step 3 and City of Nedlands Step 4
M2	Energy	The overall building and each dwelling at the top floor shall achieve at least 3.5 star NatHers rating.	City of Nedlands Step 6
M3		A suitably sized solar boosted gas centralised hot water system shall be installed.	SDC 5 and City of Nedlands Step 6
M4		Motion sensors shall be installed for lighting in basement car parks and other locations as appropriate.	SDC Step 5 and City of Nedlands Step 6
M5		Photovoltaic cells shall be installed to light publicly accessible areas.	SDC Step 5 and City of Nedlands Step 6
M6		Where supplied, minimum 3-star rated energy efficient appliances shall be installed	SDC Step 5 and City of Nedlands Step 6
M7		Draught seals or weather stripping shall be installed on openings	SDC Step 5 and City of Nedlands Step 6
M8		Water	Water efficient fixtures (for example taps and shower heads) shall achieve a minimum 3-star rated. Toilets shall be dual flush type and minimum 4-star rated.
M9	Materials	Agricultural forestry timbers shall be specified throughout.	SDC Step 5 and City of Nedlands Step 6
M10	Recycling	A screened, central waste collection space shall be included for bins for recyclable waste and other materials, or as required by the City of Nedlands, for the separation of waste at the source	SDC Step 5 and City of Nedlands Step 6
M11		Suitable provision shall be made internally for waste and recycling - i.e. two bins provided within kitchen cabinetry for recyclables and non-recyclables.	SDC Step 5 and City of Nedlands Step 6

b) Recommended items

- Capturing of cooling breezes (cross ventilation) to minimise use of mechanical cooling systems.
- Minimising daytime living areas and single aspect apartments with an orientation and openings to the south only to encourage use of natural heating through solar gain in winter.
- A suitably sized underground water tank shall be installed as a back up for the purpose of reticulating landscaped areas and will augment the standard irrigation system.

C - Precinct 8 (Lot 61): Multiple dwellings**a) Mandatory item**

The lot owner shall demonstrate a commitment to sustainability and, where possible, attempt a high level of efficiency in the following key objectives for the building's performance:

- Conservation of water (for example, through the use of minimum 3-star rated water saving devices and dual flush toilets) and energy (for example, through the installation of high star rated energy efficient appliances)
- Increase of indoor environmental quality and amenity for the occupants (for example, through passive solar heat gain in winter to living areas and openable windows for cross ventilation in summer)
- Reduction, recycling and responsible management of waste (for example, through the provision of two bins provided within kitchen cabinetry for recyclables and non-recyclables)
- Careful selection of materials (for example, with consideration given to durability, ease of construction, availability and environmental impacts).

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Dated: _____

BUILT FORM AND LANDSCAPE

The vision for Swanbourne is to create a memorable development that reflects its unique context. The elements detailed below should create a high quality living environment incorporating design principles that will help to maintain a distinctive sense of place. **Note that 'Precincts 7 and 8 – 'multiple dwellings' have additional built form requirements that are separately listed in the previous section on Site Planning.**

a) Architectural character and integrity

The building character for Swanbourne shall be distinctly contemporary and designed and constructed with architectural integrity that is of a high quality. Designs for dwellings shall reflect modern lifestyles, a neighbourly co-existence, and 'robustness' to allow for possible future changes.

Here, contemporary architecture is defined as being bold, 'edgy' and original; the clarity of appearance shall be emphasised by highly sophisticated construction detailing. The images shown below are local examples of contemporary architecture. While the examples may not wholly comply with these Design Guidelines, they do represent the architectural qualities and character required at the Swanbourne development.

The strong identity for Swanbourne shall also relate to the objectives of the development, context and site conditions, the dwelling type and modern lifestyles, and be environmentally responsive. .

The copying and direct application of imported or historical architectural colours and styles, such as Mediterranean and Federation, is inappropriate and not permitted.

Design integrity and the quality of detailing are imperative, therefore excessive ornamentation, mouldings/contrast banding, finials, classical style columns/scrolls and stick-on elements will not be permitted.

Contextual aspects to be taken into consideration in the design include:

- parks and streetscape
- surrounding existing development and land use
- topography
- microclimate
- views and vistas (subject to conditions – refer to section on Site Planning)
- orientation and ability to provide "eyes on the street" for passive surveillance of the public realm
- access by pedestrians and vehicles.



Examples of high quality contemporary architecture in Perth

Signed: _____

Dated: _____

b) Elevations

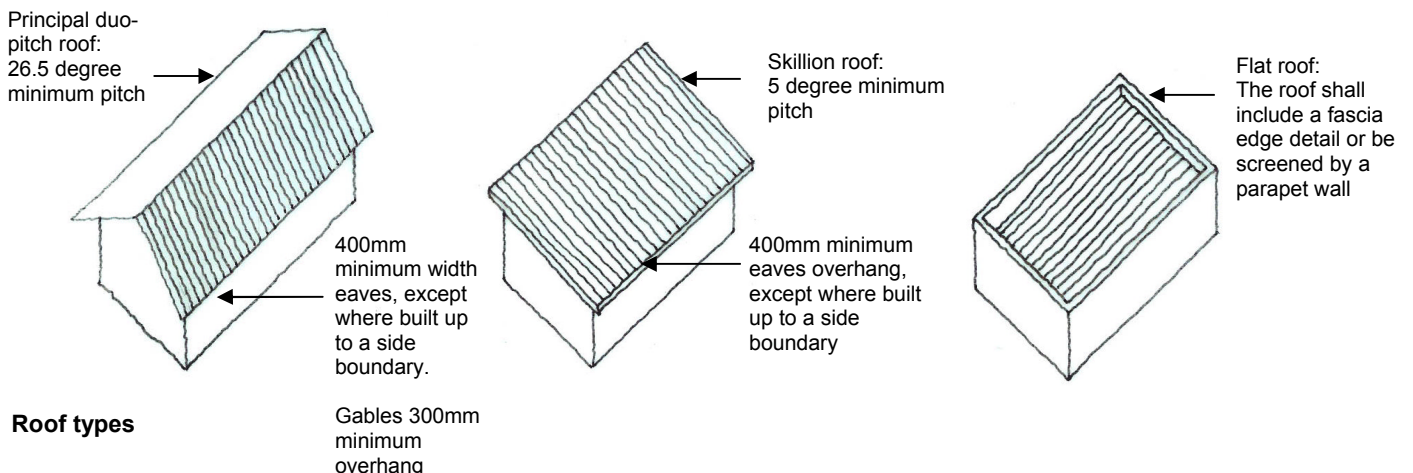
The design of a dwelling shall achieve architectural excellence and respond in an appropriate or neighbourly manner as required to the public realm. Except as noted in the Site Planning section, generally, elevations visible from the primary and secondary streets and Public Open Spaces shall include major openings and features related to habitable rooms and their associated activity, with the more private spaces and functions located elsewhere.

- A dwelling’s major public elevation shall address the primary street.
- The elevations of a dwelling on a corner Lot shall address the primary and secondary streets, or Public Open Space with openings and a consistent level of design quality.
- The elevation facing the rear shall match the quality of design of the dwelling’s major public elevations so as not to lower the amenity of neighbouring Lots.
- Any wall built up to the boundary that is visible and faces a neighbouring Lot shall be built to the same quality of finish as the primary elevation.
- Elevations shall provide interest and expression of a contemporary architectural identity and integrity through careful articulation and fine detailing, and inclusion of feature elements such as balconies, porches, openings, shade devices and clearly defined and sheltered entrances.
- Large areas and long lengths (typically no more than 9m) of unarticulated, monotone or blank wall visible from the public street are *not* permitted.
- Clear glass or clear solar control glass shall be in openings facing the primary and any secondary street. Curved, dark tinted and reflective glass shall *not* be visible from these streets.
- The garage shall be limited to one opening at 6m maximum width, or a double garage may include two openings with a central pier. A carport or parking garage parallel to the street is not permitted.
- The garage door facing the street shall be a non-patterned segmented panel type door in a muted colour.

c) Roofscape

Careful consideration shall be given to achieving an integrated roof, eaves detail, and dwelling elevation (refer roof diagram below)

- A principal duo-pitched roof shall have a minimum pitch of 26.5 degrees. Other minor roofs incorporated as part of the dwelling design may have a shallower pitch. Any gables shall include overhangs of at least 300mm.
- A principal roof, other than a flat roof and a roof built up to the boundary, shall have overhanging eaves of 400mm minimum width.
- A skillion roof pitch shall be 5 degrees minimum.
- A flat roof shall include a fascia edge detail or be screened behind a parapet wall.
- A lighter roof colour shall be selected to help reduce heat absorption (i.e. black, charcoal and dark colours, such as Colorbond Ironstone, are *not* permitted).
- A pronounced roof tile pattern will be considered on aesthetic and functional merits.



Signed: _____
 Dated: _____

d) Materials and colours

The materials and colours of a dwelling shall convey a contemporary aesthetic. Architectural richness is encouraged through the bold use of a variety of high quality materials and colours, such as:

- rendered masonry
- feature stone and limestone
- small areas of feature face brickwork
- recycled, plantation or sustainable harvested timber
- metal elements and details.

The use of alternative materials will be considered on aesthetic and functional merits.

e) Outdoor living area

- The size, orientation and location of the principal outdoor living area shall relate to at least one living area within the dwelling (other than a bedroom) by being accessible and visible from the interior.
- The outdoor living area should be located on the Lot to receive winter sunlight.
- Consideration should be given to access for daylight, air circulation and privacy. One useable outdoor area is preferred as opposed to many smaller areas unsuitable for outdoor activities.

f) Fences and letterbox**Precinct 6 (Lots 52 to 59)**

1. Any front fence facing the primary street including any corner truncation, and 4m minimum along any secondary street shall be 0.9m maximum height, or in accordance with BCA safety requirements where applicable, (excluding any retaining wall on which the fence is constructed). The fence shall be constructed in materials, colours and style to match and complement the dwelling.
2. Any fence *in front* of the dwelling and dividing Lots shall be 0.9m maximum height or in accordance with BCA safety requirements where applicable (excluding any retaining wall on which the fence is constructed). The fence shall be constructed in materials, colours and style to match and complement the dwelling.
3. The fence *behind* the front setback and dividing Lots shall be at a nominal height of 1.8m maximum (excluding any retaining wall on which the fence is constructed). The fence shall be constructed in materials, colours and style to match and complement the dwelling.

Fibrous cement (i.e. Supersix), corrugated metal sheeting (i.e. Colorbond), and timber pinelap fences are **not** permitted in this development.

Letterbox

- The letterbox shall be incorporated into the design of any front fence or, if freestanding, constructed in materials and colours to match and complement the dwelling. Australia Post's delivery requirements should be met.

Precincts 7 (Lot 60) and 8 (Lot 61)

Any fence facing the street reserve shall be maximum 1.8m height (excluding any retaining wall on which the fence is constructed). The fence shall match the design and quality of the existing solid and visually permeable Estate fence installed by Mirvac where it faces the Public Open Space.

The specification for the Mirvac fence, and a list of suggested suppliers and installers is available from Mirvac. The proposed fence design shall be submitted to Mirvac for consideration and approval prior to construction.

Existing and any proposed retaining wall

The dimensions and positions of all proposed retaining walls that are visible from public areas shall be provided to Mirvac as part of the building endorsement process, and to the City of Nedlands with the application for a Development Approval.

Any new retaining walls shall be constructed in the same materials and colours as the existing retaining walls built by Mirvac; alternative designs may be considered for approval. Copies of the engineering criteria for the existing retaining walls can be obtained from Mirvac or the City of Nedlands. No existing retaining wall or fence installed by Mirvac shall be altered without first applying for and obtaining written approval from Mirvac.

Signed: _____

Dated: _____

g) Landscape

Careful consideration shall be given to the design of the open space on the Lot and the useable outdoor living area - whether a mostly paved courtyard, elevated timber deck or larger garden with some lawn area. The landscape design should minimise water use and include appropriate structures or planting that assist with providing shade in summer and solar access in winter.

Where verges are reticulated prior to purchase, they are to be maintained by the adjoining lot owner. On lots where the verge is not reticulated prior to purchase, the adjoining lot owner shall provide reticulation to the verge as part of the landscaping work and continue to maintain the verge area.

The following is a list of recommended water wise plant species:

Tree selection

Botanical name	Common name	Mature ht (m)	Form
<i>Acacia rostellifera</i>	Summer-scented Wattle	5	shrub/tree
<i>Agonis flexuosa</i>	Peppermint	to 10	tree
<i>Allocasuarina fraseriana</i>	WA Sheoak	5 to 15	tree
<i>Arbutus unedo</i>	Irish Strawberry Tree	5 to 6	tree or shrub
<i>Banksia attenuata</i>	Slender Banksia	to 10	tree or shrub
<i>Callitris preissii</i>	Rottnest Island Cypress	to 10	tree or shrub
<i>Citris sp.</i>	Lemon/ Orange Trees	5 to 6	tree
<i>Dracaena draco</i>	Dragon's-blood Tree	6 to 10	tree
<i>Eucalyptus caesia</i>	Silver Princess	5 to 10	tree
<i>Eucalyptus erythrocorys</i>	Illyarrie	6 to 8	tree
<i>Eucalyptus ficifolia</i>	Red Flowering Gum	to 15	tree
<i>Eucalyptus macrocarpa</i>	Mottlecah	4 to 5	tree
<i>Eucalyptus torquata</i>	Coral Gum	6 to 10	tree
<i>Eucalyptus utilis (syn. platypus)</i>	Coastal Moort	5 to 10	tree
<i>Hakea laurina</i>	Pincushion Hakea	2 to 6	tree
<i>Laurus nobilis</i>	Bay Tree	10 to 15	tree
<i>Melaleuca lanceolata</i>	Rottnest Tea Tree	5	tree
<i>Melaleuca nesophila</i>	Showey Honey Myrtle	4 to 5	tree
<i>Metrosideros excelsa</i>	New Zealand Christmas tree	5 to 7	tree
<i>Olea europea</i>	Olive	4 to 5	tree
<i>Pittosporum phylliraeoides</i>	Native Willow	4	tree

Shrub selection

Botanical name	Common name	Matuer ht (m)	Form
<i>Adenanthos sericeus</i>	Coastal Woolly Bush	3	shrub
<i>Anigizanthos sp.</i>	Kangaroo Paws	0.5 to 2	shrub
<i>Agonis flexuosa 'nana'</i>	Dwarf Peppermint	0.5 to 2	shrub
<i>Calothamnus quadrifidus</i>	One-sided Bottlebrush	3	shrub
<i>Coprosma repens</i>	Mirror Bush	2 to 4	tree or shrub
<i>Cordyline australis</i>	Cabbage Tree	1 to 4	tree or shrub
<i>Diplolaena dampieri</i>	Southern Diplolaena	2	shrub
<i>Diplopeltis huegelii var huegelii</i>		0.5 to 1	shrub
<i>Eremophila glabra</i>	Tar Bush	1.5	shrub
<i>Exocarpus sparteus</i>	Broom Ballart	3	shrub
<i>Frankenia pauciflora</i>	Seaheath	0.5	shrub
<i>Gahnia trifida</i>	Coast Saw-sedge	1.5	shrub
<i>Grevillea crithmifolia</i>		1.5	shrub
<i>Grevillea olivacea</i>	Olive grevillea	1.5	shrub
<i>Grevillea preissii</i>	Marriott	0.5	shrub
<i>Gyrostemon ramulosus</i>	Corkybark	0.5-5	tree or shrub
<i>Hibbertia subvaginata</i>		1	shrub
<i>Hibbertia spicata subsp leptotheca</i>		0.5	shrub
<i>Lavatera plebia</i>	Australian Hollyhock	2	shrub
<i>Lechenaultia linarioides</i>	Yellow Leschenaultia	0.5	shrub
<i>Leucopogon parviflorus</i>	Coast Beard Heath	2.5	shrub
<i>Lomandra longifolia</i>	Spiny headed mat rush	0.5-1	shrub
<i>Lomandra 'Tanika'</i>		0.5-1	shrub
<i>Macropidia fuliginosa</i>	Black paw	1.5	shrub
<i>Melalueca cardiophylla</i>	Tangling Melaleuca	0.5 - 2	shrub
<i>Melalueca huegelii</i>		3	shrub
<i>Melalueca systema</i>		0.5 - 2	shrub
<i>Myoporum insulare</i>	Blueberry Tree	0.2 - 5	shrub
<i>Phormium tenax</i>	Flax	1 to 2	shrub
<i>Phyllanthus calycinus</i>	False Boronia	0.5	shrub
<i>Pimelea calcicola</i>		0.5 - 1	shrub
<i>Pimelea rosea</i>	Magenta Mist	1	shrub
<i>Scaevola crassifolia</i>	Thick-leaved Fan-flower	0.1 - 1.5	shrub
<i>Spyridium globulosum</i>	Basket Bush	0.5 - 5	shrub
<i>Templetonia retusa</i>	Cockies Toungue	0.3 - 4	shrub
<i>Westringia 'Jervis Gem'</i>		1	shrub
<i>Westringia fruticosa</i>	Coastal Rosemary	1	shrub
<i>Westringia dampieri</i>		1	shrub
<i>Xanthorrhoea preissii</i>	Grass Tree	4 to 6	tree or shrub
<i>Yucca elephantipes</i>	Soft-tipped Yucca	3	tree or shrub

Signed: _____

Dated: _____

Ground cover selection

Botanical name	Common name	Mature ht (m)	Form
<i>Adenanthos cuneatus</i> 'Coral Carpet'		0.3	groundcover
<i>Brachyscome iberidifolia</i>	Swan River Daisy	0.4	groundcover
<i>Carpobrotus virescens</i>	Pigface	0.3	groundcover
<i>Clematis microphylla</i>	Old Mans Beard	0.5	groundcover
<i>Conostylis candicans</i>	Grey Cottonhead	0.5	groundcover
<i>Dianella</i> 'Little Rev'		0.5	groundcover
<i>Eremophila glabra</i> 'Kalbarri Carpet'		0.5 to 1	groundcover
<i>Ficinia nodosa</i> (syn. <i>Isolepis nodosa</i>)	Knotted club rush	0.5	groundcover
<i>Gazania</i> 'Buccaneer'		0.3	groundcover
<i>Grevillea crithmifolia</i>		1.5	groundcover
<i>Hardenbergia comptoniana</i>	Native Wisteria	0.3	climber
<i>Hemiandra pungens</i>	Snakebush	0.5	groundcover
<i>Juniperus conferta</i>	Shore Juniper	0.5	groundcover
<i>Kennedia prostrata</i>	Running postman	0.2	groundcover
<i>Lepidosperma gladiatum</i>	Coast Sword Sedge	0.5 - 1.5	groundcover
<i>Leucophyta brownii</i> 'Canal Rocks'		0.5 to 1	groundcover
<i>Lomandra maritima</i>	Matrush	0.5	groundcover
<i>Orthrosanthus laxus</i>	Morning Iris	0.5	groundcover
<i>Patersonia occidentalis</i>		0.5	groundcover
<i>Scaevola anchusifolia</i>	Silky Scaevola	0.5	groundcover
<i>Scaevola crassifolia</i>	Thick-leaved Fan-flower	0.5 to 1.5	groundcover
<i>Schoenia filifolia</i> ssp. <i>subulifolia</i>	Showy Everlasting	0.4	groundcover
<i>Sollya heterophylla</i>	Blue-bell creeper	0.3	creeper
<i>Verticordia chrysantha</i>		0.6	groundcover
<i>Viola hederacea</i>	Native Violet	0.2	groundcover

Prepared by Blackwell & Associates

h) Driveways

Driveway finishes shall complement the materials and colours of the building and landscape design and be segmented to allow surface water to permeate. Approved finishes are:

- Masonry or clay paving
- Precast pavers

Alternative finishes are subject to Mirvac's consideration and approval.

Existing paths constructed by Mirvac shall not be altered in any way. Any damage caused to existing paths shall be repaired at the expense of the lot owner to match the existing colour and material. Design specifications are available from Mirvac.

Signed: _____

Dated: _____

SERVICES

The following points outline the final details for consideration in the dwelling design. These elements include pipes, TV antennae, wiring, air conditioners, drainage systems and storage areas.

These Design Guidelines establish the best way to locate items that are essential for everyday living, but may not contribute to an appealing environment. To create an attractive development we have outlined ways in which such items can be concealed or positioned in a non-intrusive way. Unless otherwise stated in the Site Planning section:

a) Services

- All pipes, wired services, clothes drying areas, hot water storage tanks and such items shall be concealed on the Lot, and shall *not* be seen from anywhere in the public realm.
- Meter boxes shall be located in the least obtrusive location from public view.
- TV antennae, satellite dishes and radio masts shall not be seen from the primary or secondary streets. These items shall be located where they are in the least obtrusive location from the neighbour's property (for example, located at the rear of the roof and below the roof ridge level away from the neighbour's outdoor living area).
- Roof and wall mounted air conditioning units are not permitted unless fully concealed.
- Refer to the section on Environmental Performance for mandatory requirements and recommendations on passive solar and energy efficiency measures.

b) Stormwater

- Stormwater shall be collected within the Lot.

c) Waste and recycling

- Waste bin/s shall be stored and concealed from public view on the Lot.
- Provision shall be made for the appropriate number of bin pads (in accordance with City of Nedlands' standard waste and recycling system requirements).

d) Storage

- Commercial vehicles, including caravans, boats, trailers etc. shall not be parked or stored on the Lot unless enclosed within a garage or fully screened from public view.

Signed: _____

Dated: _____

APPENDIX 1: PRECINCT PLAN AND DETAILED AREA PLANS

Precinct Plan

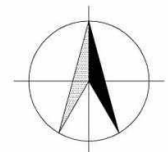


Design Guidelines A

- Precinct 1 : Lots 1 - 7
- Precinct 2 : Lots 8 - 12
- Precinct 3 : Lots 13 - 40
- Precinct 4 : Lots 41 - 47
- Precinct 5 : Lots 48 - 51
- Precinct 9 : Lots 62 - 64

Design Guidelines B

- Precinct 6 : Lots 52 - 59
- Precinct 7 : Lot 60
- Precinct 8 : Lot 61

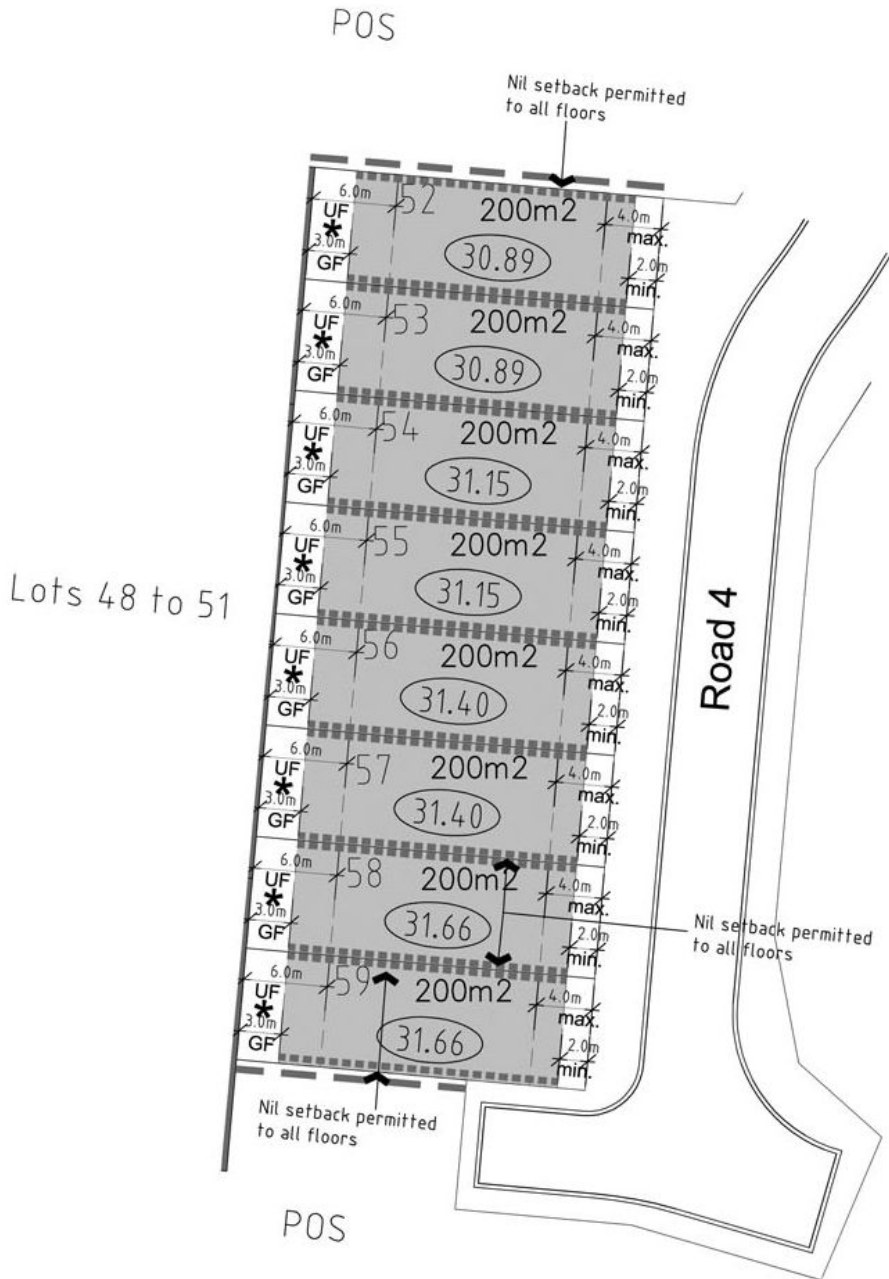


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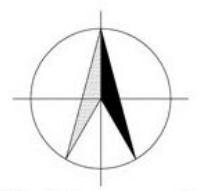
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Sheet 1: Precinct 6, Lots 52 to 59



Legend (P6):

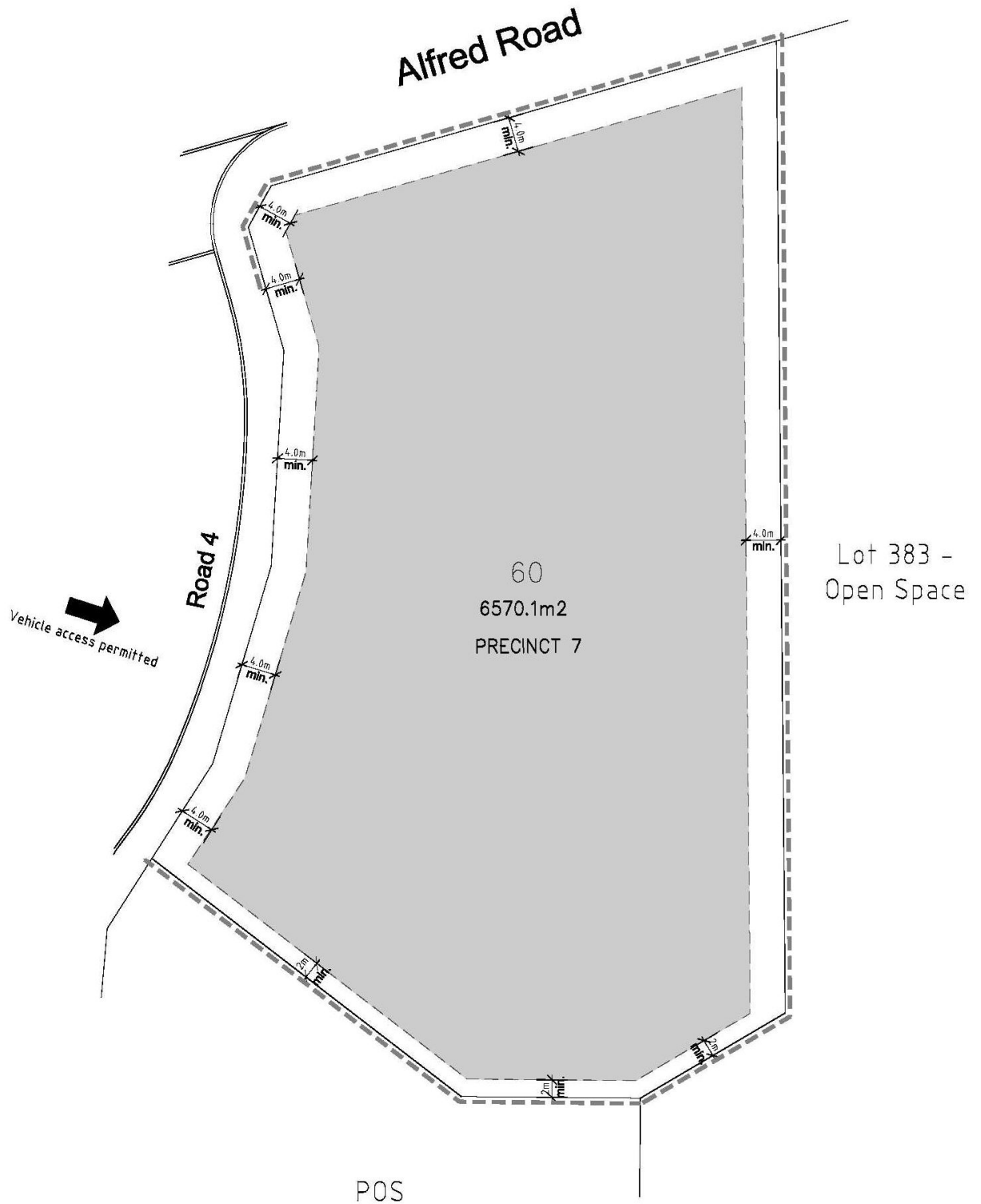
- * location of outdoor living area
- development area
- no access permitted
- nominated nil lot line location
- NB. 12m height maximum
- (30.89) pad level at completion of subdivision
- 351m² lot area



Not to scale

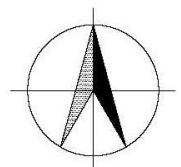
Signed: _____

Dated: _____



Legend (P7):

-  development area
-  pedestrian access only
-  351m2 lot area

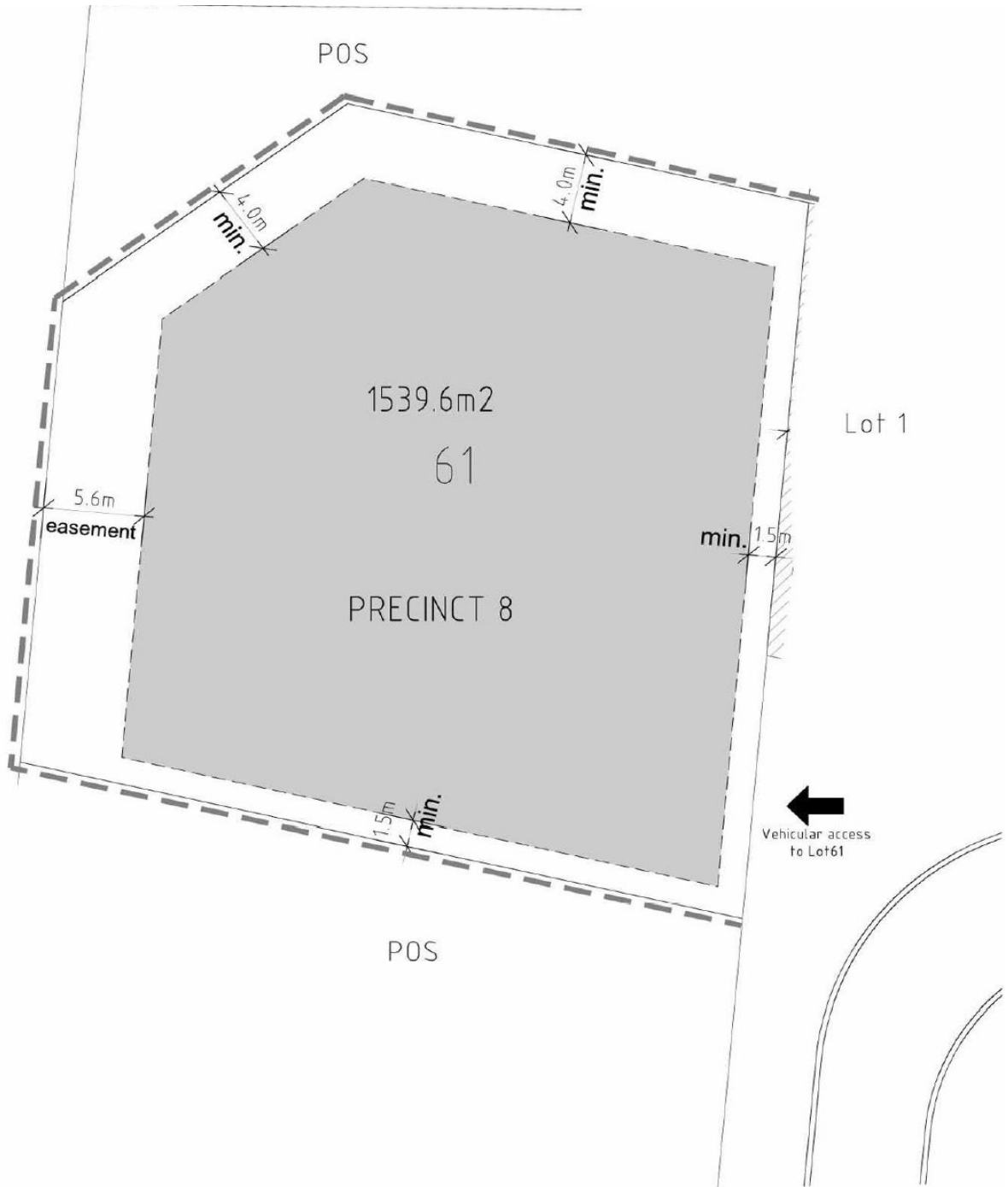


Not to scale

Signed: _____

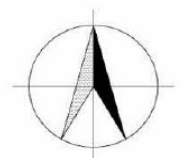
Dated: _____

Sheet 3: Precinct 8, Lot 61



Legend (P8):

-  development area
-  pedestrian access only
-  351m² lot area



Not to scale

Signed: _____

Dated: _____

DRAFT Local Hubs Framework

Version 1.1 August 2013

PD47.13 - Attachment 1 - Local Hubs Framework

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Introductory statement

In its Strategic Community Plan, Council has committed to retaining and encouraging the special character and lifestyle choices that Nedlands offers. On this basis, this plan signals Council's intent to pursue hubs spread throughout the City.

Hubs are defined as:

"A conveniently located area with a concentration of activity which provides a natural gathering point for the local community and contributes to community vitality."

It was noted that Council does not necessarily associate hubs with areas of increased density.

A Local Hubs Framework will help Council protect an aspect of the City's quality living environment specifically relating to local hubs. Through this framework a Strategy will be produced for each identified hub.

A framework dedicated to the City's local hubs will provide an overarching approach for resolving the long-term future of each of the City's identified local hubs. In this way the Local Hubs Framework will aim to create the circumstances where local residents can continue enjoying and valuing the local hubs in their neighbourhood.

Objectives of the framework

1. To create a framework/process for local hubs to be upgraded/improved.
2. To guide decision making regarding placemaking.
3. To assist in decision making regarding infrastructure.
4. To establish the guiding values of hubs within the City.
5. To define the meaning of hubs.
6. To outline a typology of hubs within the City.

Alignment with the Strategic Community Plan

The City's Strategic Community Plan includes 'strengthening local hubs/centres' as one of nine strategic priorities for Council. By completing a hubs strategy to guide the development of hubs and ensuring asset management in the local hubs contributes to their objectives, the City will aim to realise the Council's vision for:

"... lively local hubs consisting of parks, community and sporting facilities and shops where a mix of activities will bring people together, strengthening local relationships."

Local hub approach

Local hubs focus areas

The City's Strategic Community Plan identifies 11 hubs. The following characteristics have been identified for each hub: major land use, density, surrounding lot size, road network, planning devices (and if work has commenced) and classification under State Planning Policy 4.2 (SPP 4.2).

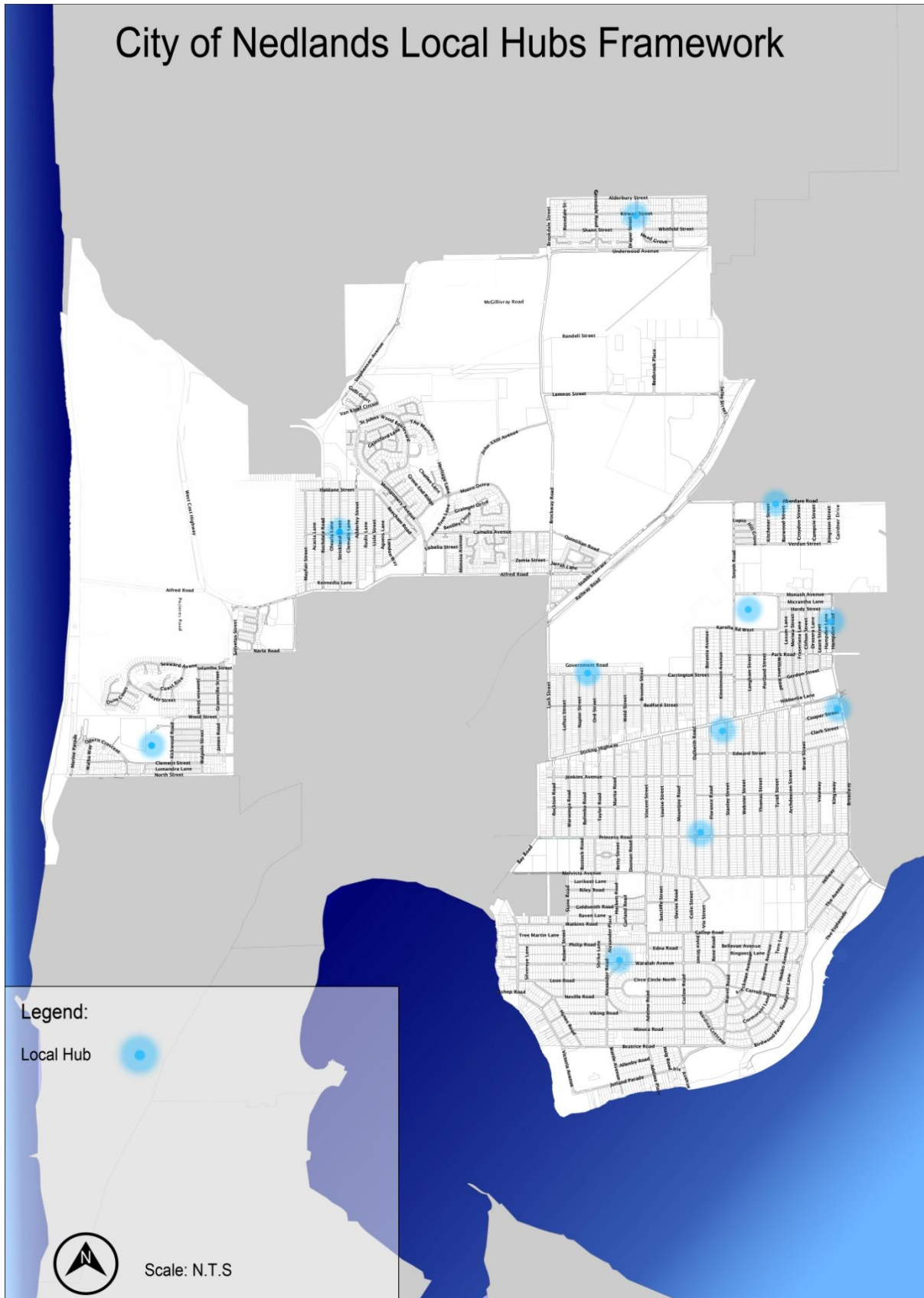
An audit has been completed on all identified hubs, the results of which have been tabulated.

PD47.13 - Attachment 1 - Local Hubs Framework

	Princess Road	Highview Park	Captain Stirling	Mt. Claremont	Hampden	Allen Park
Major land use	Commercial	Recreation	Commercial	Commercial	Commercial	Recreation
Density	Low	Medium	Low	Low	Low	Medium
Surrounding lot sizes	1000m ²	300m ² - 1100m ²	1000m ²	1000m ²	300m ² - 800m ²	300m ² - 800m ²
Road network	Traditional grid	Traditional grid	Traditional grid	Traditional grid	Traditional grid	Curvilinear
Planning devices	Nil	Highview Park Masterplan	Stirling Highway Special Control Area Provisions	Housing Diversity Area	Hampden Study Area Directions 2031	Swanbourne Masterplan
Work commenced	No	Yes	Yes	No	Yes	Yes
SPP 4.2 classification	Local centre	N/A	Neighbourhood centre	Local centre	Neighbourhood centre	N/A

	Broadway	Waratah Avenue	Croydon Street	Carrington Street	Floreat
Major land use	Commercial	Commercial/Community	Commercial	Light Industrial	Commercial/Residential/Recreation/Community
Density	Low	Low	Low	Low	Low
Surrounding lot sizes	1000m ²	1000m ²	1100m ²	700m ²	920m ²
Road network	Traditional grid	Traditional grid	Traditional grid	Traditional grid	Traditional grid
Planning devices	Broadway Study Area Directions 2031	Waratah Avenue Placemaking Strategy	Nil	Nil	Nil
Work commenced	Yes	Yes	No	No	No
SPP 4.2 classification	Neighbourhood centre	Neighbourhood centre	Local centre	Local centre	Local centre

The following map shows the distribution of hubs across the City.



Hub typologies

There are three distinct types of hubs based upon their dominant use. The main hub types are commercial, recreational and other.

Commercial

The majority of the hubs within the City are based around commercial activity. By nature commercial uses gather, this increases convenience and allows for shared infrastructure. Within the City there are six commercial hubs:

1. Princess Road;
2. Captain Stirling;
3. Mt. Claremont;
4. Hampden road;
5. Broadway;
6. Croydon Street.

Recreational

Recreation is a large part of urban and suburban life; therefore multi-use recreational areas are natural community gathering points. Within the City there are two identified recreational hubs:

1. Highview Park; and
2. Allen Park.

Other

Unlike commercial or recreational hubs, there are some hubs that do not rely on a single use but rely on a mix of uses that can include community, light industrial, commercial and recreational. Within the City there are three hubs that fit into this category:

1. Floreat;
2. Carrington Street; and
3. Waratah Avenue.

Develop criteria for managing future hubs

New hubs may emerge in the future or an existing hub may grow beyond expectations and require reclassification.

To recognise an area as a new hub the table following outlines a list of factors and accompanying criteria to assist in assessing the potential hub.

PD47.13 - Attachment 1 - Local Hubs Framework

Factor	Criteria
Community need	Current and projected demand for community facilities and services.
	Projected population increases.
	Key demographic factors including proportions of aged, children, youth, etc.
	Availability and use of existing facilities.
	Support for community hub from local community.
Accessibility	Public transport services
	Cycling and walking accessibility
	Parking (either available or capable of being provided)
Public gathering	Adjacent to public space or potential to incorporate public space in design.
Integration	Proximity to local shopping and activity centres
	Potential for integration with adjacent compatible uses
Potential for strong partners	Existing community organisations, resident and neighbourhood groups, state government agencies, service organisations/clubs, local businesses, other councils, religious organisations.
	Availability of other sources of funds through project partners.
	Levels of social capital and community involvement within the community.
Potential for service collaboration and/or coordination	Existence of services with willingness to work cooperatively in a potential hub.
	History of cooperation and collaboration in the community.
	Other plans and projects in the area.
Availability and opportunity	Council either owner of the site or be aware of a process to secure tenure.
	Land acquisition opportunities.
	Development potential – zoning.
Financial viability	Availability of funds for capital cost of building and/or fit out of the facility.

Source: Feasibility Study of Community Hubs for the Parramatta Local Government Area – Briefing Paper

Making it happen

What is Council's role?

Council has prioritised the management of local hubs in the Strategic Community Plan. Council will be required to create the necessary conditions that will enable the vision set out in the Strategic Community Plan to be realised. This will include;

- Council ensures community consultation is carried out in a meaningful way.
- Council as a key land owner in many of the hubs should act as a leader to manage and develop these assets in an appropriate manner.
- Council creates an administrative environment that allows for transparency and accountability without stifling economic development.

The Local Hubs Framework sets out Council's preferred strategy and process for ensuring the City's hubs meet the needs of all users.

Model for developing hub plans

The model taken for the improvement of local hubs is dependent on the size of the hub in relation to State Planning Policy 4.2 *Activity Centres for Perth and Peel* (SPP 4.2). Under SPP 4.2, Activity centres are classified in relation to their size. The City of Nedlands only has the two smallest categories are neighbourhood centres and local centres (local centres being the smallest). The hubs are classified as follows:

Local Centre	Neighbourhood Centre
Princess Road	Captain Stirling
Mt. Claremont	Hampden Road
Croydon Street	Broadway
Carrington Street	Waratah Avenue
Floreat	Highview Park
	Allen Park

Neighbourhood centres

The model used for the improvement of local hubs classified as a Neighbourhood Centre is that of a steering committee. For each hub project, a steering committee will be formulated consisting of Councillors and residents.

The primary function of each steering committee will be to:

- Determine the matters and guidelines that should be included within the strategy for each local hub.
- Guide the decision making process.
- Act as a link between the community and the project team.

Approval responsibilities for the steering committee:

- Identification and prioritisation of project objectives and outcomes;
- Confirmation of a detailed project scope statement; and
- Budget constraints.

Steering Committee membership will include:

- Two ward councillors.
- Up to three local residents/stakeholders.

Local centres

If the hub is classified as a local centre under State Planning Policy 4.2, a steering committee will not be formed. Hubs classified as a local centre are considerably smaller in nature. Concepts will be prepared in accordance with the objectives and principles of this document. Stakeholders will be consulted as per the consultation plan outlined in this document

Stakeholder consultation

The City is committed to undertaking robust and comprehensive community engagement with its community to facilitate a constructive dialogue on key decisions and projects.

As per the Council's Community Engagement Policy, the City will be required to prepare a Community Engagement Plan at the planning stages for developing each hub. At this stage of the project, it is premature to develop these as each hub will vary in terms of size, use and community expectation.

Responsibility

The Community Engagement Plan will be developed and carried out by the Community Engagement Coordinator and the project working team.

Objectives of community engagement

- Ensuring that the City and its community are informed on the project.
- Ensuring that where identified that the City and the community are consulted and involved in the project using the best methodologies and practices as identified in the community engagement plan.

Undertaking community engagement

As per the Council Community Engagement Policy, the City will use four different levels of Community Engagement adapted from the International Association for Public Participation (IAP2). The level required to be used for each hub strategy will be identified when developing the community engagement plan.

Below are the four levels used by the City

Inform	Informing the community of decisions and services	To provide the community with information to assist them in understanding the problems, alternatives and/or solutions. Keep the community informed of the issue and decision.
Consult	Consulting the community as part of a process	To obtain public input on issues and/or decisions; to acknowledge and consider public concerns.
Involve	Involving the community through a range of mechanisms	To work directly with the community to determine public concerns and opinions and ensure that these are directly reflected in the alternatives developed and decision made.
Collaborate	Collaborating with the community by developing partnerships	To work in partnership with the community on each aspect of the decision making process, including understanding of the issues, developing alternatives and identifying the solution.

Some of the tools which will be used in community engagement for the Local Hubs Strategy will include but are not limited to:

- Letterbox drop
- Surveys
- Advertisements in local newspapers
- Task force/working group
- Media
- Newsletter
- Special publications
- Briefings
- Focus Groups
- Signs

- Meeting called by the community
- Public Meeting
- Website
- E-consultation
- Community Feedback Register
- Community information sessions
- Notice boards and information posters
- Attendance at community group meetings
- Open House

Implementation and scheduling

General principles

The following set of general principles has been created to implement over all hubs within the City. These guiding principles will help achieve the desired outcomes for this strategy. In conjunction with the general principles, typology specific principles will also apply.

a) Placemaking and urban design

Good urban design provides a sense of well being and amenity by ensuring recognition of the natural context and the functional requirements of the community. This can be achieved by:

- Public spaces that are lively and pleasant;
- A feeling of safety/security;
- Uncluttered and easily maintained streets;
- Integration of public art;
- Well designed lighting and street furniture;
- Places suited to all demographics; and
- Attractive and suitable planting.

b) Legibility

Legibility establishes an understanding of place and wayfinding for users of the hub. Enhancing the legibility of a hub will include the identification and incorporation of elements such as:

- Natural landmarks and focal points;
- View corridors;
- Clear and easily navigated routes;
- Clear edges/buffers;
- Lighting; and
- Signage/wayfinding.

c) Connectivity

Ease of movement for users of hubs is reinforced by consideration of connectivity and permeability. Services and facilities should be accessible to all users this includes a range of ages and consideration for disabled access. Connectivity can be improved by:

- Concentration of uses near public transport;
- Hubs are well connected to roads/footpaths;
- Easily accessible;
- Availability of information;
- Choice of transit options; and
- Direct routes to/from.

d) Character

A hub that enhances the distinctive local landscape, sense of place and history whilst providing a quality living environment will establish a rich character unique to its locality. A hubs character can be built upon/established by the identification/enhancement of the following:

- Distinctive natural features;
- Locally distinctive built form;
- Spaces of natural/cultural significance;
- Building materials;
- Avoiding standard solutions

Typology related principles – Commercial

a) Community use integration

The potential for multi-use/multi purpose hubs should be investigated. The co-location of community uses with already frequented areas provides opportunities for the City to explore.

b) Street level activation

Commercial areas benefit from street level activity. Activation of streets fronts promotes character and a friendly environment. Activation can be improved by al fresco dining, tenancies addressing the street and ensuring the street level is not dominated by services.

c) Scope for housing diversity

Commercial areas provide an opportunity to increase housing diversity. By collocating commerce and housing both are promoted. This can be achieved by increasing density around commercial areas or providing housing on top of existing commercial.

d) Allow for shared facilities

The collocation of uses also results the collocation of facilities. Commercial hubs can benefit for the sharing of facilities including parking, landscaping and services.

Typology related principles – Recreational

a) Involve community groups

By their very nature, recreational hubs rely heavily on community groups such as sporting clubs.

b) Modern up-to-date facilities

A key aspect of successful recreational hubs will be that they have modern up-to-date facilities that attract people and allow clubs to be successful. These facilities allow for the needs of a wide range of people to be catered for.

c) Flexibility

Allow for flexibility for a range of uses including active and passive recreation.

Typology related principles – Other

a) Co-locate or cluster community uses

Greater coordination of service delivery as co-location promotes closer ties and more collaborative working relationships.

b) Respond to, and are shaped by, the unique circumstances and needs of the local community

Addressing local community needs and supporting and developing community strengths is fundamental. The requirement for hubs to be responsive to their communities is a critical success factor in their development.

c) Retain mix of uses, with a focus on local living, that attract people at different times of the day.

A mix of uses is important to ensure that hubs serve multiple needs and have a positive influence in communities.

d) Active street fronts

Commercial areas benefit from street level activity. Activation of streets fronts promotes character and a friendly environment. Activation can be achieved by al fresco dining, tenancies addressing the street and the street level not being dominated by services.

e) Involve community groups

Involving community groups ensures that hubs are identified as focal points of the community.

Budgetary principles

The following broad principles apply to the process of planning to strengthen existing hubs and the implementation of the framework:

- 1) The focus of all work must be value for money.
- 2) A budget to undertake the planning process to strengthen each hub identified in this framework must be available before the process commences.
- 3) A budget must be established for each hub prior to the commencement of that hubs planning.
- 4) Work is to align with the commitments outlined in the Strategic community Plan.
- 5) The recommendations flowing from the planning process must be tested for affordability before a final plan is recommended for adoption.
- 6) Affordability can include maximising grant opportunities, public/private partnership and other revenue raising options.

Priority

Many of the hubs identified within the Strategic Community Plan are already involved in or have already been part of strategies for their future. The following table details this:

Hub	Planning devices	Status
Highview Park	Highview Park Masterplan	Year 1-3 in Corporate Business Plan (2013-2017)
Allen Park	Swanbourne Masterplan	Year 3-5 in Corporate Business Plan (2013-2017)
Princess Road	Nil	-
Croydon Street	Nil	-
Floreat	Nil	-
Mt. Claremont	Housing Diversity Study	Study Complete, project ongoing
Hampden Road	Hampden Road Study	Ongoing
Broadway	Broadway Study	Ongoing
Waratah Avenue	Waratah Avenue Placemaking Strategy	Ongoing
Captain Stirling	Stirling Highway Special Control Area	Approved Draft

Hub	Priority
Hampden Road	1
Broadway	2
Waratah Avenue	3
Highview Park	4
Allen Park	5
Captain Stirling	6
Mt. Claremont	7
Floreat	8
Princess Road	9
Croydon Street	10

Priority will be given to hubs that are involved in ongoing projects, followed by those without existing planning projects.

In some cases, projects have already been initiated and have commenced. These projects will draw from the Local Hubs Framework principles but not processes.

Relevant legislation

This document sits below a range of legislation from both state and local government, creating a suite of legislation for local hubs.

- State Planning Policy 4.2 Activity Centres

Monitor and review

Reviewing this Framework is important to ensure that it remains relevant.

Objectives

- To ensure that the overall Framework is remains relevant.
- To ensure the content of the overall Framework is accurate.

Outcomes

- A review establishes whether or not the aspirations, objectives and principles of the Strategy still aligns with community needs and expectations and relevant Council and State Government documents.
- A review confirms that the recommendation of the Strategy is still appropriate.
- The content of the Strategy is up-to-date.

Time frame for review

- In five years a minor review of the overall Framework is to be completed.
- In ten years a major review of the overall Framework is to be completed.

Proposed Dog Registration Fees as per Dog Amendment Regulations 2013

Item	Fee	Current Amount	Proposed Amount
1.	Inspection of register	\$0.50	\$0.50
2.	Certified copy of an entry in the register	\$1.00	\$1.00
3.	Annual registration, unsterilised dog or bitch, unless a concessional rate applies.	\$30.00	\$50.00
4.	Concessional registration rates –		
	a. Sterilised dog or bitch, annual registration	\$10.00	\$20.00
	b. Guide dogs	Nil	Nil
	c. Dogs used for droving or tending stock	25% of fee otherwise payable	Same
	d. Dogs owned by pensioners referred to in regulation 4(2)	50% of fee otherwise payable	Same
	e. Foxhounds, bona fide kept together in a kennelled pack of not less than 10.	\$40.00 per pack	Same
	f. Registration after 31 May in any year, for that registration year.	50% of fee otherwise payable	Same
	g. Three year registration period –		\$42.50
	A) Sterilised dog or bitch	\$18.00	\$120.00
	B) Unsterilised dog or bitch	\$75.00	
	h. Lifetime registration period –		\$100
	A) Sterilised dog or bitch		\$250
	B) Unsterilised dog or bitch		
	i. Dogs kept in an approved kennel establishment licensed under section 27 of the Act, where not otherwise registered	\$100 per establishment	\$200 per establishment
	j. Dogs certified by the Director of the State Emergency Services as being tracker dogs used for the purposes of the State Emergency Services in the specified registration year	\$1.00	\$1.00

PD48.13 - Attachment 1 - Updated Schedule of Fees

Proposed Cat Registration Fees as per Cat Amendment Regulations (No.2) 2013

Item	Fee	Amount
1.	a. One year registration of a cat	\$20
	b. Pensioners	\$10
	a. Three year registration period	\$42.50
	b. Pensioners	\$21.25
	a. Lifetime registration period	\$100
	b. Pensioners	\$50
	a. Registration after 31 May in any year, for that registration year	50% of fee payable otherwise
2.	Annual application for approval or renewal of approval to breed cats (per cat)	\$100